

GRAIN DEALERS' JOURNAL

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CHICAGO, ILL., DECEMBER 25, 1899.

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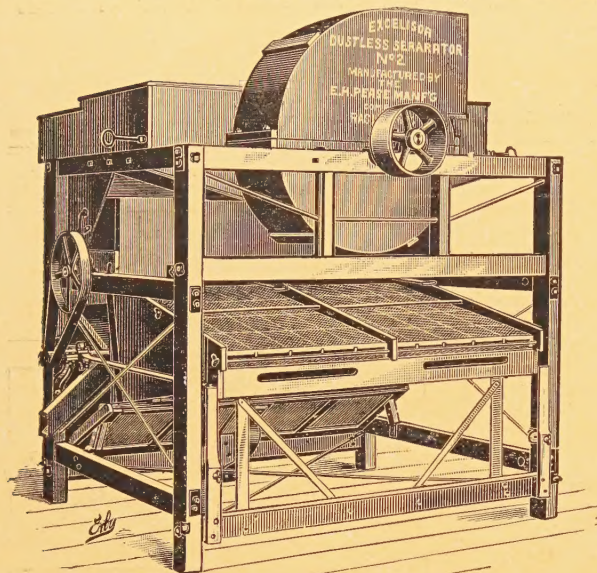
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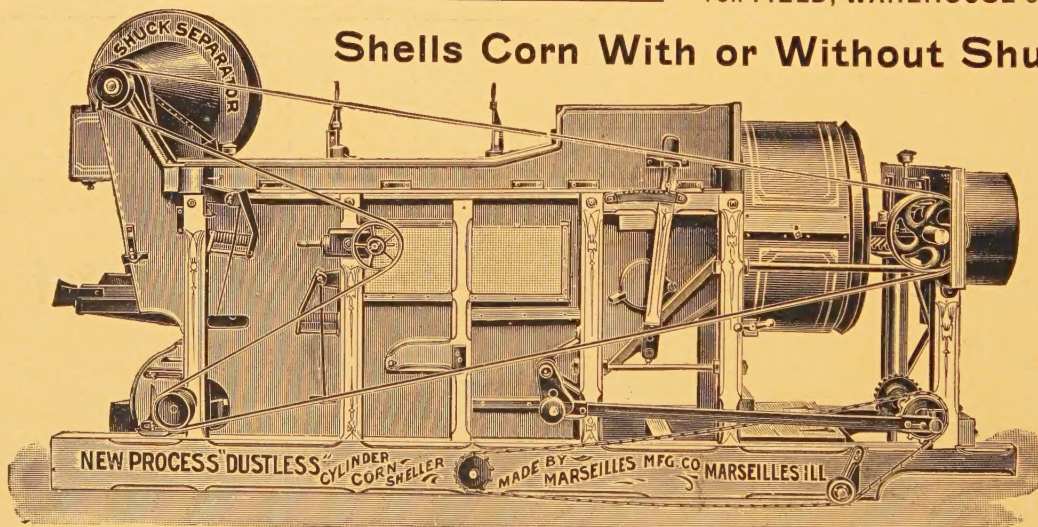
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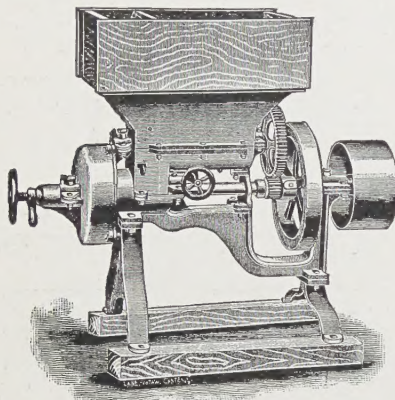
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Separating Wheat and Oats, Corn and Oats, Wheat and Flax.

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Write for Circulars and Descriptions of our Different Machines.

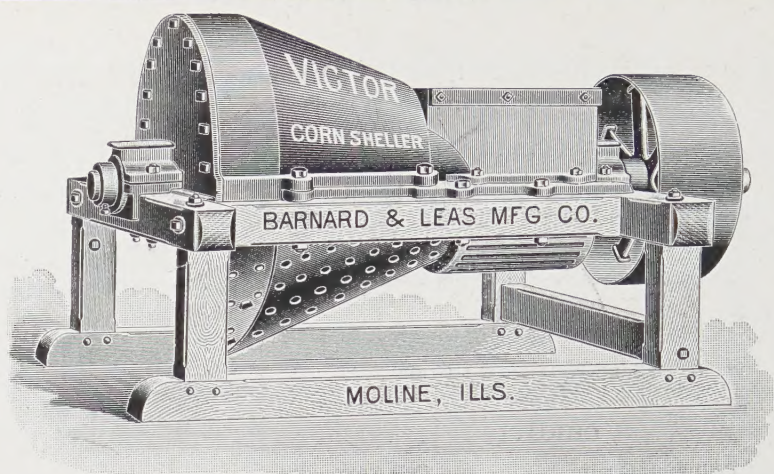
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Only 1-8 to 3 Horse Power Used. We build this machine in eight different sizes, capacities ranging from 50 to 2,500 bu. per hour.

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LITTLE VICTOR COMBINED SHELTER AND CLEANER.

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BARNARD'S IMPROVED
DOUBLE AND SINGLE SCREEN DUSTLESS CORN CLEANERS

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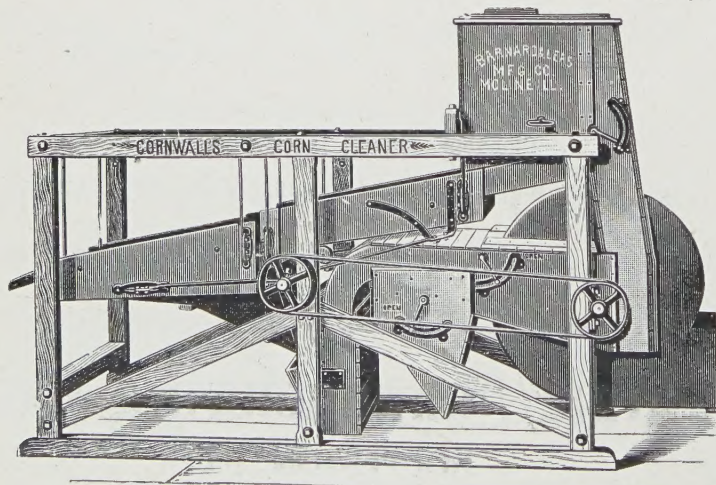
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A combined sieve and air machine. Never clogs; cleans cleaner than any other; corn never grades dirty; once through does the work; saves screenings for feed; is dustless; runs light; is durable. Sieves are adjustable. Cleaner can be used for other grains.

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IS THE
**Only
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4. In which the heat given off by the cooling grain is saved and utilized."
5. In which all parts are removable and interchangeable, a small wrench only being necessary.
6. Which has been officially approved by the Chicago Underwriters' Association.
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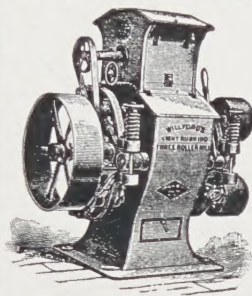
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ARE THE BEST FEED GRINDERS FOR ELEVATORS



Willford's Three-Roller Mill

BECAUSE.....

They have the greatest capacity for power consumed
They have solid cast frames.
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They have noiseless belt drive.
They have given satisfaction to thousands of users all over the United States.

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These Machines will operate successfully and rapidly at a temperature as low as 110 degrees. Practical millers and elevator men will appreciate this.

IT IS ONE THING TO KILN DRY
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OF GRAIN INTO ITS NORMAL CONDITION
BY NATURE'S OWN METHOD.
WE CAN DO IT.

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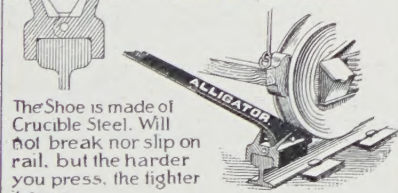
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Try an
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Pinch Bar.**

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IF NOT SATISFACTORY, NO SALE.

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Crucible Steel. Will
not break nor slip on
rail, but the harder
you press, the tighter
it grips.

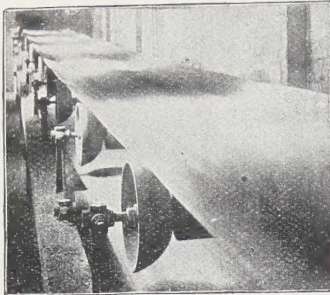
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It is very simple and nothing
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raise lever till front end
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In ordering, give the size of rail.

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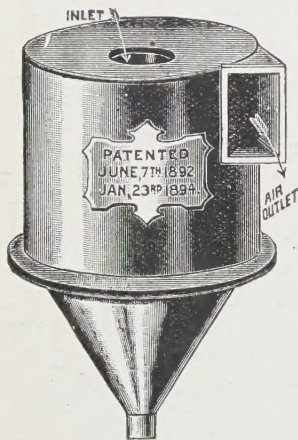
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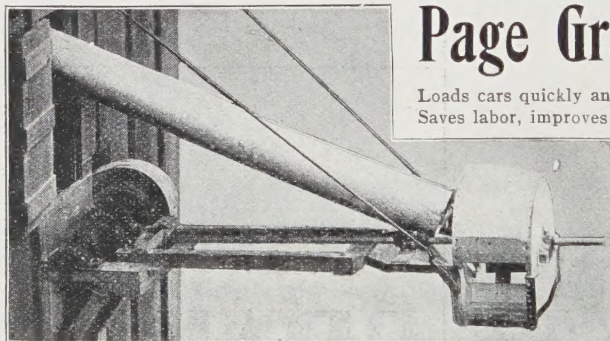
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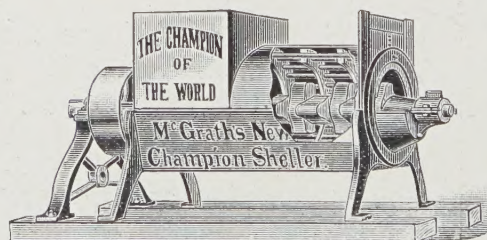
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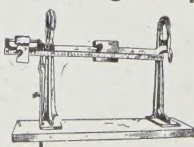
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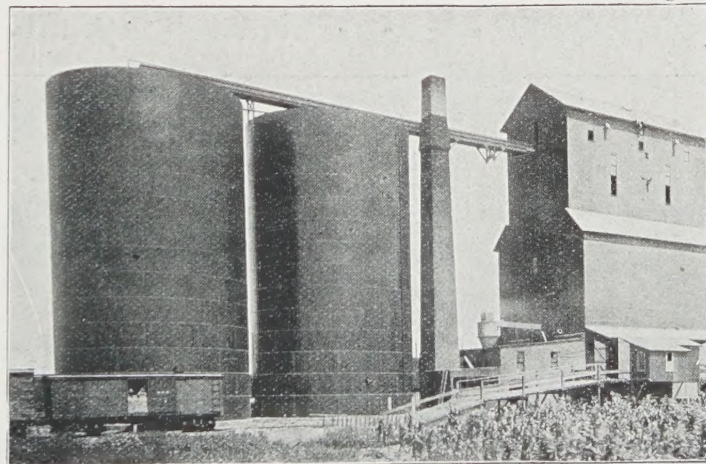
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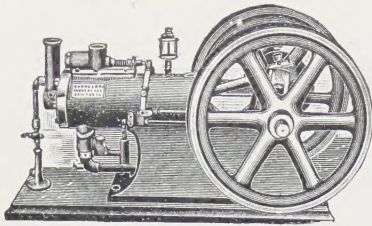
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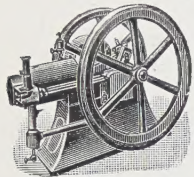
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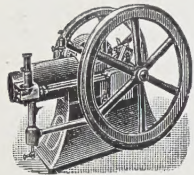
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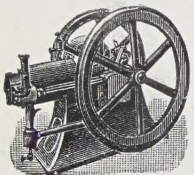
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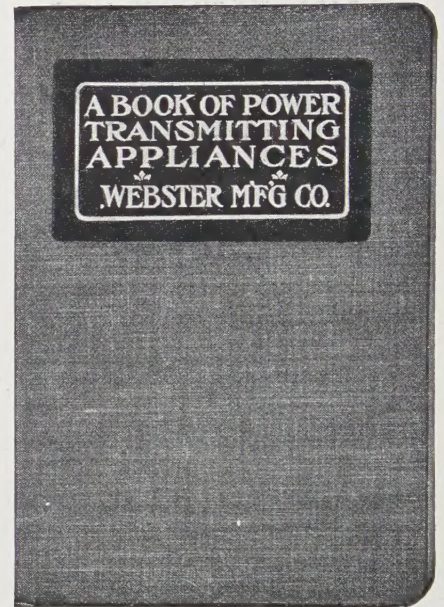


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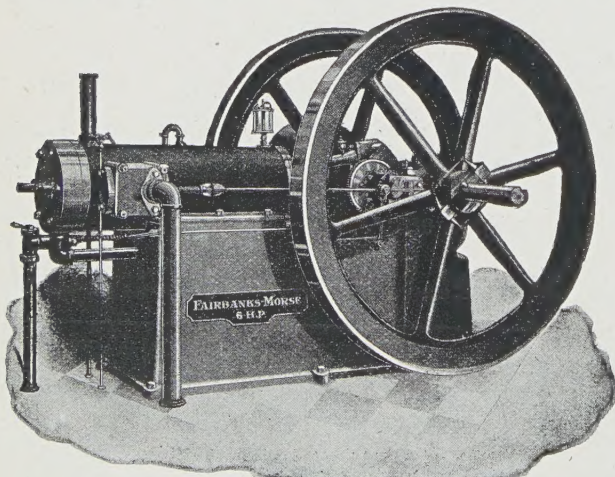
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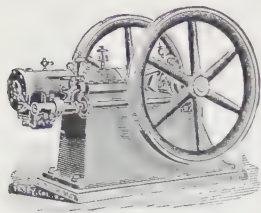
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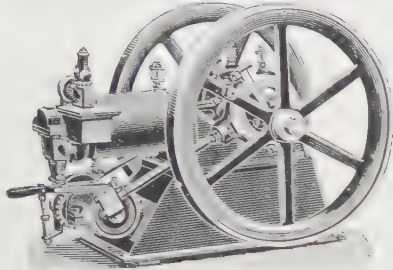
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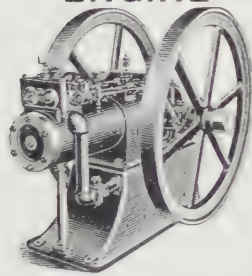


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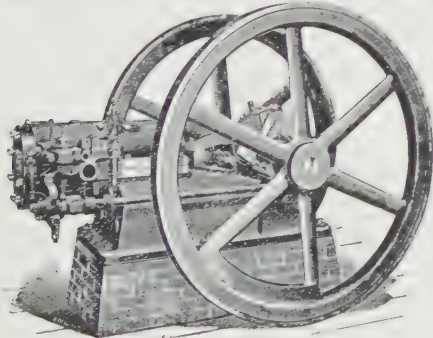
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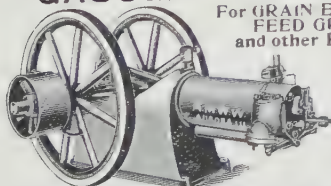
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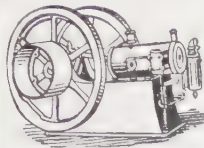
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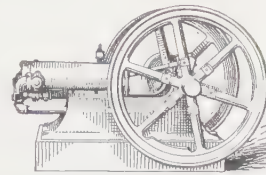
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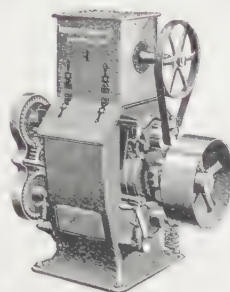


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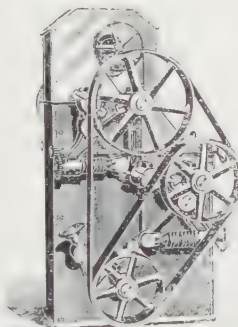
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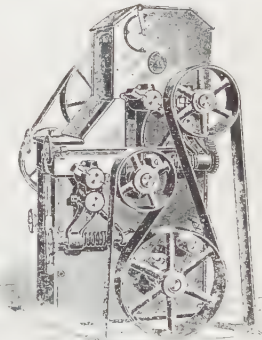
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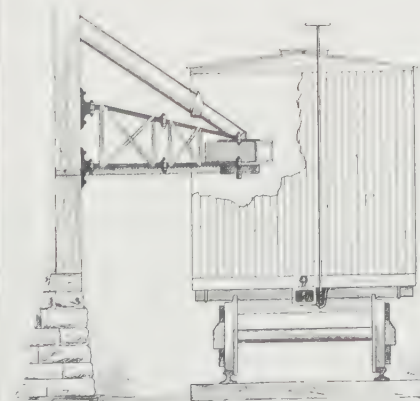
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CHOICE lots of seed or grain can be sold at premium by advertising here. Try it.

SEEDS OR GRAIN of special grade or variety can be obtained by advertising your want here.

NEW, home grown timothy, clover and millet seed for sale. J. R. Smith & Son, Lamoni, Iowa.

SPRING wheat samples wanted, with prices on 5, 10 and 20-bu. lots. Jos. E. Boardman, El Paso, White Co., Ark.

OATS wanted. Red rust-proof, free of Johnson grass; also big-yielding pure white oats. E. Schiff Co., Greenville, Tex.

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HALF interest for sale or exchange in 50-bbl. flour mill in nice Missouri town on Santa Fe; paying business. Address Frank M. Smith, Rutledge, Mo.

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TO LET.—Space in this department, to elevator owners who wish to let

The grain crops in Sardinia this year are almost a total failure. Many of the people are reduced to the extremity of subsisting on wild figs and bread made of acorns and barley.

FINE LOGATIONS**FOR ELEVATORS, FACTORIES AND OTHER INDUSTRIES****BEST SWITCHING FACILITIES. COMPETITIVE RATES. AMPLE CAR SUPPLY.****Write for further information to B. THOMAS,****Pres. Belt Ry. of Chicago, Dearborn Station, CHICAGO.****Clark's Record
OF CARS SHIPPED.**

Is adapted for the use of country grain shippers. To keep a record of all cars of grain shipped. This book is 9½ x 12 inches and has space for 2500 cars. The leaves are made of ledger paper, ruled for date sold, date shipped, car number, initials, to whom sold, destination, grain, grade sold, their inspection, discount, amount freight, our weight, bushels, destination bushels, over, short, price, amount freight, other charges and remarks. It is well bound in strong board covers, with leather tack and corners. **Price \$1.50.**

GRAIN DEALERS COMPANY
10 Pacific Ave., Chicago, Ill.

Table of Legal Weights.

THIS TABLE shows the legal weights in pounds per bushel of grain and grain products, etc., as provided for by law. This book is well bound in a strong cloth cover. **Price 50 cts.**

For sale by
GRAIN DEALERS COMPANY,
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ADOLLARWELLINVESTEDISWORTH TWENTYSAVEDSUBSCRIBETODAY**GOOD INVESTMENT TRY IT**

GRAIN DEALERS COMPANY,
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Gentlemen:—Enclosed find One Dollar for which please send the

GRAIN DEALERS' JOURNAL on the 10th and 25th of each month, for one year to

Name _____

Post Office _____

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State _____

Clarks Grain Tables

AVOID UNNECESSARY FIGURING,
PREVENT ERRORS IN COMPUTATIONS
BY USING

CLARK'S STANDARD SERIES
OF GRAIN CALCULATORS,
FOR REDUCING POUNDS TO BUSHELS.

Clark's Grain Tables for Wagon Loads reduces team scale weights to bushels. This book is prepared for use by Country Buyers. It contains 9 tables, showing the number of bushels in any load from 100 to 4,000 lbs. The first table is for reducing weight of oats to bushels of 32 lbs.; the second is for oats at 35 lbs.; the third is for barley, Hungarian grass and cornmeal at 58 lbs. to the bushel; the fourth is for shelled corn, rye and flax seed at 56 lbs. to the bushel; the fifth is for wheat, clover seed, beans, peas and potatoes at 60 lbs. to the bushel; the sixth, seventh and eighth are for ear corn at 70, 75 and 80 lbs., respectively, to the bushel; the ninth is for timothy seed at 45 lbs. to the bushel. All of the tables are printed in heavy faced type on good paper. The price of this book, bound in strong manilla cover paper, is 50 cents.

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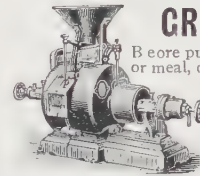
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Before purchasing a mill for grinding feed or meal, or anything that can be ground on a mill, write us for catalogue and discounts. Guaranteed and shipped on trial.

Manufacturers of French Burr Mills, Corn Shellers, Crushers, and full line of Flour Mill Machinery.

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GRAIN INSURANCE.

Special attention to Open Floater Policies in the best Stock Companies.

Insurance follows grain up and down as the quantity stored in each house changes. Will always have insurance where you have grain.

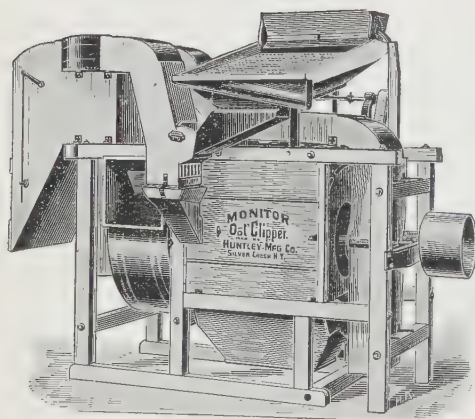
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H. H. LANTZ & CO., - - DES MOINES, Iowa.
25 years' experience. Best of references.

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Nickel Plate Elevator,

Operated by HARRY G. CHASE, 10 Pacific Ave.
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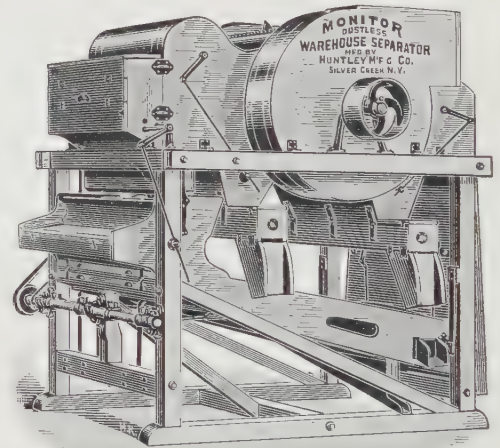


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Barley, Corn, Flax
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**OAT CLIPPERS,
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GRAIN DEALERS JOURNAL

Published on the 10th and 25th of each month

BY THE

GRAIN DEALERS COMPANY.

10 PACIFIC AVE., CHICAGO, ILL.

CHARLES S. CLARK, Editor.
J. CARVER STRONG, Advertising Representative.
NORMAN H. CAMP, Attorney for the Company.

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Letters on subjects of interest to those engaged in the grain trade, and news items are always welcome.

CHICAGO, ILL., DECEMBER 25, 1899.

Cutting commissions is becoming so very general that it seems probable that cutting will work its own remedy.

The car loaders seem to be gaining in popularity and assist in reducing grain handling to a mechanical operation.

We wish all of our friends and patrons a prosperous New Year. May it bring them much business and fair profits.

There has not been an explosion of elevator dust for some months, yet no grain elevator man seems disposed to declare such disaster impossible.

A number of vessels recently pressed into service of the British government as army transports will soon be relieved, and again engaged in transporting grain.

We are indebted to George Broomhall, publisher of Corn Trade News, Liverpool, for an original New Year's card bearing the British and American flags and his hearty good wishes for 1900.

The Hessian fly seems determined to make trouble for wheat growers in Ohio, where it has done great damage to successive crops. It is one pest which scientists seem as yet unable to hold in check.

A great many dealers who will this week attempt to determine the amount of profit accruing from the year's business will find frequent cause to speak unkindly of the railroad companies, and especially of the carrier which has delayed grain at initial point and in transit.

The next meeting of the Grain Dealers' Union of Southwest Iowa and Northwest Missouri will be held at Council Bluffs January 9th. This very successful association is extending its territory and is sure to receive a warm welcome wherever dealers are well enough posted regarding its past work

to have a clear conception of its ability to help them.

St. Louis weights are again receiving much needed attention. It is a credit to the St. Louis grain merchants that they recognize the defects of the system and are working to remedy same. They have the earnest wishes of the entire trade for success.

The dealer who does not strive to cultivate the acquaintance and friendship of his competitors; who does not exert himself to conduct his business so as to remain on friendly terms with them, invariably courts unreasonable overbidding and trouble.

Turn over a new leaf. Begin the new year by reading your trade journal carefully and by telling your neighbors of the good things you found therein. Contribute news and letters to its columns. Help make it the grain dealers' journal in the true sense of the word.

The buyer who believes his competitor is paying $2\frac{1}{2}$ cents over what he can afford, simply because three or four farmers in succession tell him the same story, has something to learn regarding the way farmers enjoy to play upon the credulity of the average grain buyer.

A receiver, who also buys on track, stipulates in his bid, "To avoid mistakes, state prices and amounts in telegram of acceptance." How many misunderstandings would be avoided if all shippers would take this precaution. More trouble is due directly to careless methods than to all other causes combined.

Many dealers have been without cars for months, yet those very dealers decline or neglect to support any association or any grain trade publication. If they would start in the new year by supporting both, we feel certain that the balance for 1900 would be far more likely to be on the right side of their ledger.

The railroads have already aroused the western shippers to protest against the uncalled for rise in rates and changes in classification. A number of prominent hay men met in Chicago recently and protested most vigorously against the changing of the classification of hay which will increase the cost nearly \$1 per ton to those filling old contracts.

An Indiana farmer's paper, quite contrary to the general rule, recently published an article blaming a farmer because the grain buyers would not take his grain at any price. It seems that this farmer, just like the majority of the grain growers, bought cheap seed corn, little of which grew. He had to replant, which resulted in his crop be-

ing so late as to be caught by frost, and the grain moulded on the cob. It pays to use first-class seed grain, and it would pay the grain dealers to make an effort to supply his farmer patrons with such seed grain.

An Iowa buyer who is much worried by the high prices paid by his competitor writes: "I propose to meet his prices for good corn, but I will cut the price so low on the poor stuff that the farmers will take it all to him." His business shrewdness is likely to cause his competitor a few restless nights. Harmony in that market would insure a living for both of them.

The practice regarding the disposition of track sold grain which fails to grade is still far from uniform, but most track buyers seem disposed to do what is fair in the disposition of grain which fails to grade upon arrival, and some of them are now specifying in their bids "grain failing to grade will be applied at market difference day of arrival, unless otherwise agreed."

Several of the grain dealers' associations have on different occasions adopted resolutions condemning those who neglect to pay amounts due other dealers. The Texas and the National Associations have gone even farther than this and offered to arbitrate where any difficulty arises between dealers. Some shippers seem to neglect just bills, until the creditor is justified in concluding that the dealer proposes to beat him out of the amount. The influence of association work has been to establish a higher grade of business ethics and it is natural that the associations should take the high ground in these matters that they have. It gives the organizations better standing in the trade, and assists in bringing about the permanent retirement of dealers not worthy to be classed as regular.

The Indiana Farmer, in response to a query from a Carroll county subscriber, says: "The law of 1897 makes 70 pounds of ear corn a bushel until the 1st of December each year; after that date 68 pounds." What a fickle law! What a useless attempt to legislate value into corn cobs and water. It is known to all persons who have anything to do with grain that it will shrink, and the Indiana grain buyers are shrewd enough to know that they cannot afford to pay corn prices for water and realize on it when shipped to another state. Comparatively no part of the state's corn crop is shipped to points within the state. All of the central markets are without its boundary. To protect their working capital from the avaricious farmers and the farmer legislators, the Indiana dealer has long since ceased to buy corn by the bushel.

Some of them give so much per hundred-weight; others give so much per 70 pounds. They have nothing whatever to do with bushels. Either way enables them to get around the requirements of the law.

The Standard Oil Co. has not yet seen fit to reduce the price of gasoline to a fair figure, yet the country elevator men continue to place gasoline engines in their houses regardless of the cost of gasoline. In the corn surplus states many seem still to prefer the steam engine and continue to buy the standard make which has been so popular with elevator men for many years.

Some of the Montreal papers are very much worried by the privileges granted the Connors Syndicate. They seem to be convinced that it will result in the grain business being monopolized, and the extermination of dealers heretofore doing business at Montreal. They complain that the exchange, insurance and freight will all be engaged at point of shipment, and they are right in their claim, but there seems no reason why the established dealers of Montreal should for any reason whatever retire from business and give the Connors syndicate a free field.

The elevator man who wastes steam and gasoline in pulling bucketless belts over pulley know without any computation that the operation is an expensive one, yet there are many country elevator men who seem to overlook the fact that it is just half as expensive to operate an elevator which has but one-half of the buckets attached. It will take such a leg twice as long to elevate a given quantity of grain as it would if it had the full number of buckets. The power required to elevate a given quantity of grain with such a belt is of course much greater than it would be were the belt properly equipped with buckets. It is a comparatively easy matter to keep elevator legs in good working order, and surely it would be found a profitable investment by those who will take the time and trouble to have same.

Railroad companies seem to be getting more arbitrary each month. The new rule affecting the loading of cars to capacity which they say they will put into force January 1st is sure to cause a world of trouble to the grain trade. The carriers insist that if a car is marked 40,000 pounds capacity, it must be loaded to capacity, or else freight paid on that amount regardless of what amount is placed in the car. This rule is sure to cause much trouble to the trade. Many track buyers as well as various markets have rules governing what shall constitute a car of wheat, corn, oats and other grains, all

of which must be ignored if the new rule is to be tolerated. The railroads seem disposed to be arbitrary in so many freight matters of late years that it is probable that they will draw much antagonistic legislation down upon themselves unless they become more considerate of the shipper's interests soon.

Ohio and Iowa dealers are agitating the organization of other associations and no doubt the dealers of both states will profit by the movement. It is to be hoped that the promoters of these new associations will profit by the experience of the trade and depend for success upon the local divisions. Through them alone can the dealers who come in direct competition with each other be brought together at frequent intervals without great expense of time and carfare. Then, too, the matters handled by the local divisions are generally of sufficient interest to all members of the division to attract all to the meetings and hold their attention. None of their time is taken up by the discussion of matters in which they are not directly interested. It is also much easier to get all the dealers of a district who come in competition with each other, in a local division. It serves, first of all, to get such competitors acquainted with one another. Friendship overcomes prejudices, unreasonableness and generally results in the maintenance of fair prices.

There seems to be some misunderstanding regarding the meaning of the last ruling of the Commissioner of Internal Revenue regarding the use of stamps on grain tickets. Some of the quibbling bankers are disposed to insist upon keeping the money deposited by the grain buyer for payment of tickets separate from other money on deposit. According to the opinions of different collectors, this was not the intention of the Commissioner. All that is necessary is that the buyer deposit money with the banker or merchant for the express purpose of paying his grain tickets. If his balance has been paid out or his ticket is presented by a third party, then it must bear a two-cent stamp, the same as a check. The ticket of W. S. DuBois, which was published on page 343 of the Grain Dealers Journal for December 10th, has been accepted by other representatives of the revenue department than the one who approved Mr. DuBois' ticket. The correspondence regarding its first use by him is published in this number. The form is so simple and the requirements so easy on both buyer and banker as to seem to recommend its use to other dealers.

LETTERS FROM THE TRADE

[Regular grain dealers are invited to contribute letters on grain trade subjects, for publication in this department.]

HARD TO AVOID THE TAX.

Grain Dealers Journal: I have read the articles in the Journal giving the forms of grain tickets which do not need stamps. This method of making the ticket payable to original holder only and keeping a separate account seems to be a way around the stamp tax, but rather circuitous and one hard to follow. Banks would not care to keep an account that they had no use of and just had to hold for one purpose and distinct from all other moneys. We have not spoken to our bank yet, but I hardly think they would care to handle an account that way. Perhaps a better way might be to work it through some merchant. I do not know just what use we can make of the idea, but will find out, and am very much obliged for the information. E. E. Ellsworth, Johnson City, Tenn.

GRAIN TICKET APPROVED BY INTERNAL REVENUE DEPARTMENT.

Grain Dealers Journal: In using my form of grain ticket, which was reproduced in The Journal for Dec. 10, page 343, I have money on deposit to pay the checks, and it can be used for no other

J. W. Patterson, Collector Internal Revenue, Third District of Iowa Dubuque. Dear Sir:—We hand you herewith sample of the grain ticket prepared for the use of one of our grain merchants. In your opinion does his meet the requirements? Yours respectfully, F. P. Huff, Cashier.

First National Bank, Rockwell City, Ia. Gentlemen:—Replying to your letter of Dec. 1 I will state that in my judgment the style of grain ticket attached with your letter can be used without revenue stamps in paying for grain. Yours truly, J. W. Patterson, Collector.

purpose. To get a ruling on the legality of this method of paying and style of ticket, the cashier of the bank sent a sample to the Internal Revenue Collector of this district. The correspondence is given herewith. W. S. DuBois, Rockwell City, Ia.

LONG AND SHORT SCALES.

Grain Dealers Journal: I was much amused at Mr. Caldwell's letter on the relative benefits of long 22-foot platform wagon scales compared with 14-foot scales. His theory reads well, but if he will buy grain in the country, as I have done the past thirty years, and watch his shortages closely, he will come to the conclusion that his shrinkage is more on the long scales than on the short ones.

The reasons of this are very easily understood, when you come to think, that when a team pulls a big load of grain several miles, about the first thing they do when they stop in the elevator is to empty themselves of twenty to thirty pounds of urine and manure, which the country grain man has already weighed as grain, as the horses are weighed with the load. This is so common that all grain men of any experience know it to be true without discussion. The extra expense of maintaining the long scales, saying nothing about their extra initial cost, is no small item. The longer the scale the easier it is for them to get out of level, and to

make a scale weigh correctly they must of necessity be level.

Let country grain men arrange the approach to their scales with the side on which the horses stand while the load is being weighed as high as the scales for a distance of ten feet, and I will guarantee that no trouble will be experienced. We have long ago thrown out most of our long scales, and want no more of them at any price for our business. R. G. Risser, Kankakee, Ill.

STAMP REQUIRED IF CHECK CHANGES HANDS.

Grain Dealers Journal: Herewith we give the form of ticket we use at country stations to issue to farmers for

much of this corn to go to the drying establishment as possible, as they then get enough of kiln dried corn to insure them against any reclamations from their European customers.

The Grain Dealers' Association of Illinois should have a representative at New Orleans at once to look into the grading of corn at that port, and protect the interest of the association. The crop of corn in Illinois is better in quality than it has been for ten years during the month of December, and still there has never been so much complaint regarding the grade of corn at New Orleans as there is at the present time. The grades are unusually and unreasonably severe, and Illinois shippers will do well to keep shy of that market

lic storage system of Chicago, the control of markets by public warehousemen, several of whom also do a large private wire speculative business, car shortages, cut freight rates to favorites, and numerous other forms of injustice and oppression by railroads, increased competition by buying on track bids, and the baneful effects generally of track selling. These are only a part of the great questions that grain dealers' associations should consider, instead of consuming time with an occasional irregular shipper.

While I feel most friendly to grain dealers' association and would much enjoy meeting old friends at their gatherings and in trying to help the good cause along, I belong to none and attend no meetings, for the reason that I am not a grain shipper, though deeply interested in their success. Such associations would do more general good if all commission men, track buyers, railroad men, public weighmasters and inspectors were denied membership and excluded from business meetings, except upon special invitations.

This is a period in the country grain trade when none but Americans should be placed on guard or admitted to council deliberations. Grain dealers' associations are composed of men of known ability for any work, and they should have opportunities to labor in their own way for their own advancement, uninfluenced by the presence of or personal regard for those whose business duty is to spy upon the works of individuals and associations and to defend and maintain objectionable customs and abuses. E. W. Burdick, Chicago, Dec. 22, 1899.

\$	CARRGIL ELEVATOR CO.		No.
PAYER AT			
Station		1	
Pay to			or order,
		DOLLARS,	
For	Bu.	Lbs. Gross.	
For	Bu.	Lbs. Dockage.	
For	Bu.	Lbs. NET.	No. Wheat. Price
		Price	
Not good unless properly endorsed.			BUYER.

grain. We take a contract from our payer which reads that the money is ours and is to be kept separate. It is not necessary to stamp checks when the money is kept separate and where money is paid direct to holder; but if the check changes hands it has to be stamped. Cargil Elevator Co., Minneapolis, Minn.

CORN INSPECTION AT NEW ORLEANS UNSATISFACTORY.

Grain Dealers Journal: There is a great deal of complaint about the irregularity of the inspection of corn in the City of New Orleans at the present time. Some of the new corn from Illinois is grading No. 2, some shelled out the same day from the same parties No. 3 and some No. 4.

Several of the grain men have gone to New Orleans to look after their inspections, but they find the Inspection Department at New Orleans is indirectly if not directly under the control of the men who buy the corn for export at that port, and have also found that the majority of the Appeal Committee are representatives of prominent export firms in that city. It is almost impossible to have any changes made on cars when once they have been inspected No. 4.

The shrinkage, switching, cost of kiln drying No. 4 corn aggregates from 3 to 4 cents per bushel, and when the corn is kiln dried, it is of course much better corn than any of the dealers have made contracts to furnish. The card bids from the New Orleans exporters read No. 3 corn or better. It seems, therefore, that it is to the interest of the New Orleans exporters to compel as

for the present. Respectfully, R. G. Risser, Kankakee, Ill.

SUGGESTIONS FOR ASSOCIATIONS.

Grain Dealers Journal: It has been so long since I had direct knowledge of farmers or scoop shovel men shipping grain that it seems to me as if the business had entirely ceased, and yet the main question discussed at every meeting of grain shippers is, how to prevent such irregular business? As consignments by such shippers are seldom solicited or wanted by the best class of commission firms, it often seems as if shippers were pulling this string altogether too much in association meetings and wasting time that could be better employed in considering more important issues.

I have had experience in buying grain in competition with farmers and scoop shovel men, and during my ten years of traveling (calling on grain dealers and millers exclusively) I learned the experience of many others, and it is my experience that such competition is the weakest that a veteran dealer can have. Only recently the Grain Dealers Journal told of a country shipper who had been forced out of business by strong competition, and if it had been done by irregular shippers the Journal doubtless would have said so. The strongest competitor in grain business is one who thoroughly understands his occupation, most certainly not one who does not.

Some of the most important abuses in the grain trade, and which can only be remedied by dealers in association, are in the inspection and weighing departments in central markets, the pub-

REPORT NAMES OF UNFAIR COMMISSION HOUSES OR TRACK BUYERS.

Grain Dealers Journal: If any of the trade in Illinois is troubled by commission houses or track buyers not using the list as compiled and corrected by our association, it would be justice to themselves and the rest of the trade if they would send in the names of any such firms, with all the details, promptly; so that our executive committee could thoroughly investigate such cases and report to the next annual June meeting of our association in 1900, in order that the trade may know exactly what is going on around them in this state. E. R. Ulrich, Jr., president Illinois Grain Dealers Association, Springfield, Ill.

Great Britain eats her entire wheat crop in about thirteen weeks.

It is rumored and denied that Secretary Wilson, of the Department of Agriculture, will resign on account of illness.

The only wheat contract inspected today (Dec. 8) was eight cars from local cleaning houses. There was no No. 2 white oats in 170 cars of white received, and in 293 cars of corn only 14 graded contract. With such inspections, is it profitable to build expensive elevators in the country and equip them with cleaning and grading machinery? If we were handling grain in the country and had a scoop shovel that would not make grain grade better than modern country elevators, judging from Chicago inspections, we would throw it away and use a wooden snow shovel.—E. W. Burdick, Chicago.

ASKED AND ANSWERED

LIABILITY FOR OVERLOADING CAR.

Grain Dealers Journal: Is the shipper or the railroad company liable for damages resulting from the overloading of a car with grain? F. M. S.

ADDRESS OF WESTERN GRAIN CO.

Grain Dealers Journal: Can anyone give me the names of the officers of the Western Grain Company and their addresses? The company operates on the line of the Illinois Central railroad, in Iowa. T. E. Malden, Manson, Ia.

HAS IOWA'S LANDLORD LIEN LAW BEEN SUSTAINED?

Grain Dealers Journal: Can anyone inform me whether or not the Supreme Court of Iowa has passed on the constitutionality of the landlord lien law of Iowa? I know it has been passed upon by the lower courts, but doubt that the Supreme Court would sustain it. Derby Elevator Company.

Date.....1900
Load of
From
Price per cwt....., bu
Gross
Tare
Net

CLAIM FOR FAILURE TO FURNISH CARS.

Grain Dealers Journal: With regard to collecting from the railroad company the advance in freight when cars are ordered ahead of the advance, I want to know which railroad I should put in my claim against—the road I am situated on or the road I intended the grain to go over last? S. A. Muff, New Carlisle, O. Ans.—The initial carrier.

REQUISITION FOR CARS.

Grain Dealers Journal: We noticed in the Grain Dealers Journal a way of aiding one to get cars by sending written notice to carriers demanding cars. We would like to have a form for such notice, as we have been hindered on account of the scarcity of cars. William Drew & Sons, Delisle, O. Ans.—All that is necessary is a formal written notice to general freight agent. Telegrams to him frequently bring cars before refused. A formal written demand for cars, acknowledged before a notary, will bring them when telegrams fail.

WHAT POWER IS REQUIRED TO MOVE CAR.

Grain Dealers Journal: We are thinking of placing a siding over which cars will have to be moved by horse power. The maximum grade is five feet to the hundred, and the distance one thousand

feet in all. Will some reader of The Journal kindly advise us through its columns the number of horses, or the pulling power, that will be required to move a car, the loaded weight of which will average 75,000 pounds? Blue Grass Commission Co., Lexington, Ky.

IS FORM TAXABLE?

Grain Dealers Journal: We inclose herewith sample of check used in our elevator when paying farmers for grain delivered. Notice the manner in which we fix the check on the back, so that the farmer can draw his money. We write on the back the number of hundred-weight of corn and the price per hundredweight underneath it. Then we carry out the multiplication to determine the total amount due farmer. Immediately below this we write the word "Approved" and sign our name. We tell the farmer to get his money at the store of Roe Bros. Nobody can draw the money except the man to whom the grain belonged. Please inform us whether or not we have to place stamps on this form of check. At present we do not. Beaty & Doan, Ossian, Ind. Ans.—As this check makes the amount pay-

GRAIN CHECK. Beaty & Doan Elevator.

Date.....1900
Load ofGross.....
FromTare.....
Price per cwt....., bu.....Net.....

able only to the person to whom issued and out of a fund kept solely for the purpose, it comes within the last ruling of the Internal Revenue Commissioner and does not require a stamp.

Turkey is to be warned to stop discriminations against American flour.

In Norway farmers give a Christmas to the birds. A sheaf of wheat is fastened to the top of a tall pole and set up in the farmyard on Christmas day.

A railroad is to be built in Russia to connect the cities of Orenburg and Taschkent. The line will follow the left bank of the Ural river and cross Turkistan.

The Christmas number of the Saturday Evening Post marks a new departure in periodical literature—the first successful attempt to give, for five cents, stories, articles and pictures by the same writers and artists who make the high-cost magazines. For example, the opening story in the Christmas Post is by Rudyard Kipling, and the tale that of Private Otheris and his dog, Garm; Joel Chandler Harris tells "Why the Confederacy Failed," a stirring story of the Secret Service; and Ian MacLaren, Justin McCarthy, M. P., John Luther Long, M. E. M. Davis, W. C. Coup and W. S. Harwood contribute stories and articles. The verse in the number is by Edwin Markham, Frank L. Stanton, Mary E. Wilkins and Clinton Scollard.

MEETING OF SOUTHEAST IOWA DEALERS.

A meeting of the Southeast Iowa Grain Dealers Association was held at Burlington on the evening of December 18.

President J. A. Carden, of Winfield, Ia., called the meeting to order at 8:35 p. m. and asked Secretary McClurkin to read the minutes of the previous meeting, held November 13. The minutes were read and approved.

Secretary McClurkin called the roll. It being understood that if any member had any complaint to make of the conditions existing in his territory, to do it when his name was called. About thirty were present, and some entered complaints. The most serious trouble complained of was that the feeders were taking all the corn and left none for the dealers to ship.

President Carden: I was in Chicago last week and called upon Mr. Clark, the secretary of the Grain Dealers National Association. He told me of many new points which have arisen lately in connection with placing revenue stamps on grain checks, and I asked him to talk to you on this subject. We will now hear from Mr. Clark.

Mr. Clark told of the taxes levied upon the country grain buyer and different methods adopted for avoiding payment of the tax on grain tickets; also of the recommendations made by different collectors which would exact more revenue from the grain buyers; of the protest made by the National Association against such changes and of its petition for a reduction of the grain dealers' tax. Different forms not requiring a stamp were exhibited and their use explained. The following new ruling by the Internal Revenue Department was read:

Grain tickets and the like may be cashed by a regular employee of the company using the same, and directly to the parties to whom they are issued, without liability to the stamp tax; and they may also be cashed by a person not a regular employee of the company issuing the same, provided the company deposits money with said person for the specific purpose of cashing these tickets, and providing the tickets are cashed out of the buyer's own money and no other.

Mr. Clark explained that the last requirement, "and providing the tickets are cashed out of the buyer's own money and no other," was proving burdensome to the bankers and merchants, who were cashing the grain tickets, and recommended that the move being made by the National and other associations to have this requirement waived be supported by the Southeast Iowa association.

Mr. McClurkin moved that a committee of three be appointed to draft a resolution in keeping with Mr. Clark's recommendation.

The motion was carried and President Carden appointed E. L. McClurkin, J. A. Baxter and E. A. Miller.

President Carden: We have not yet secured the required number of members to consummate affiliation with the National Association. We need only five more. I am confident it will prove of advantage to every dealer to join in this association and I am at a loss to understand why any should hesitate for a minute. I would like to see the affiliation completed.

D. K. Unsicker, Wright: I am confident dealers will profit by joining the National.

E. L. McClurkin: I was shown a letter from the division freight agent of

the C. R. I. & P. railroad recently, which showed that the road which has heretofore been supposed to be against our association is for us and will assist us in inducing the dealers along its line to join. For this change of front we are indebted to the secretary of the National association. The National can help us in many other ways, but unless we support it we cannot expect it to work for us. The benefits derived from attending the National Association meetings greatly exceed the cost of membership. The regular dealer who refuses to join makes a mistake.

J. A. Cardin, Winfield: I attended a meeting at Des Moines in June, 1897, of the National Association and there learned from the address of Chief Inspector Noble, of Chicago, something regarding reinspection of grain which has saved me hundreds of dollars.

Miles Doran, Nichols: I shipped corn to Chicago, was notified that it was graded No. 3. I wired for reinspection and it was graded No. 4.

H. H. Riepe, Sperry: I am sorry to say that I was delegated to attend the National meeting, but I failed to go. I promise you that if ever I get another chance to attend I will do so.

A recess was then taken for the members to get acquainted with one another and they improved the opportunity.

The second session was called to order by President Carden and the following report of committee to draft resolution was read and adopted:

Whereas, The last ruling of the Treasury Department requires that where unstamped grain tickets are cashed by other than the employees of the grain buyer, they must be "Cashed out of the buyer's own money and no other," and

Whereas, This requirement inflicts an unnecessary burden upon the banker or merchant who pays the ticket for the grain buyer; therefore be it

Resolved, That we petition the Treasury Department and the Commissioner of Internal Revenue to amend the requirements so that the grain tickets may be paid from any funds, providing the money is paid to the party to whom the ticket is issued.

J. O. A. BAXTER,
E. A. MILLER,
E. L. McCLURKIN.

President Carden: It looks like the feeders would absorb all the corn and that the elevator men will handle none of it. If we could devise some means of remedying the trouble it would be welcomed by the elevator men. Now is the time we should act.

John Baxter: If we raise the price, we will induce the feeders to ship in corn, so the railroads will get more business.

D. K. Unsicker: If they ship in corn and we get the home grown corn, the railroads will get the freight on the home grown corn out, the corn into the feeders, and the live stock out.

John Baxter: I would sooner handle corn for nothing and be doing something rather than be sitting around and doing nothing. The railroads could better afford to carry our grain for a low rate and be doing something.

H. H. Riepe, Sperry: Let us put up the price, make the feeders pay more for what they do use, and discourage their shipping in any more steers.

E. A. Miller, Packwood: I think there is not enough margin in corn to enable us to raise the price enough to overcome the feeders' price. The feeders are paying more than Chicago prices. Unless the railroads will carry out the corn deadhead we cannot get it.

Joe Barton, Roscoe: I have an elevator and am feeding some steers. I think you

will find that the feeders will pay thirty-one, two or three cents. They have to have the corn and will pay the price necessary to get it.

G. H. Carter, Pekin: I would like to see something done to enable us to get business.

C. Fye, Mt. Union, moved that the governing committee be empowered to arrange for a raise in the price of corn so as to enable the dealers to get some of the corn.

The motion was carried.

President Carden: There is another matter which I would like to have brought up. The legislature meets this winter and I think we should do something to have the landlord's lien law changed. You may not have had experience with the law, but my experience cost me \$176. I think we should ask our representatives to have the law amended so that the tenant will be held criminally liable for selling grain against which there is a lien, and the landlord be required to record his lien.

Joe Barton: I had to pay for oats a second time.

Several other members reported trouble with landlords.

Charles S. Clark, secretary of the Grain Dealers National Association, spoke of the efforts being made in other quarters of the state to secure a remedy for the trouble. The grain dealer has acted as guardian and protector of the landlord's interests entirely too long. It is time they were seeking relief from the burdensome duties. The Iowa code provides as follows:

Sec. 2992. Landlord's Lien. A landlord shall have a lien for his rent upon all crops grown upon the leased premises, and upon any other personal property of the tenant which has been used or kept thereon during the term and not exempt from execution, for the period of one year after a year's rent, or the rent of a shorter period, falls due; but such lien shall not in any case continue more than six months after the expiration of the term. In the event that a stock of goods or merchandise, or a part thereof, subject to a landlord's lien, shall be sold under judicial process, order of court, or by an assignee under a general assignment for benefit of creditors, the lien of the landlord shall not be enforceable against said stock or portion thereof, except for the rent due for the term already expired, and for rent to be paid for the use of demised premises for a period not exceeding six months after date of sale, any agreement of the parties to the contrary notwithstanding.

The following extracts are from decisions bearing on the law:

A lien exists upon crops raised by the tenant, and such crops may be followed by the landlord into the hands of the purchaser; *Holden v. Cox*.

The lien of the landlord can be enforced against a purchaser from the tenant of property which in the ordinary course of business of the tenant is kept for use and not for sale, such as the team of horses used in cultivating a farm; *Richardson v. Petersen*.

Where the landlord knows that grain raised on the premises and covered by his lien is being sold to an innocent purchaser and makes no objection, he will be estopped from asserting his lien as against such purchaser; *Wright v. Dickey Co.*

One who, with knowledge of the fact, buys from the tenant corn produced upon the leased premises, such corn having been sold by the tenant without the knowledge of the landlord, is liable to the landlord for the value of such corn to the extent of the landlord's lien thereon; *Evans v. Collins*.

The sale or transfer of property which is subject to a lien will not affect the lien; *Neeb v. McMillan*.

E. L. McClurkin moved that a committee be appointed to confer with the other associations of the state and select a committee to present the matter to the proper legislative committee. Carried.

The chair appointed D. K. Unsicker, Wright; Charles Fye, Mt. Union, and H. K. Smith, Olds, as such committee.

At midnight a motion to adjourn was carried and the dealers dispersed to their hotels.

CONVENTION NOTES.

The attendance was light.

As usual, the meeting was held in the office of R. C. Jordan.

Among the railroad men in attendance were R. Law, manager of the B. & N. W., and C. P. Rorbach, traveling freight agent of the B., C. R. & N.

R. C. Jordan was there, but having put on twenty pounds of flesh since the spring meeting, some of the dealers failed to recognize him.

The association's field of usefulness is not half developed. The members would profit by learning of the work done by other associations. Then would they have a clearer idea of the possibilities within their own territory.

Among the dealers present were: Joe Barton, Roscoe; J. A. Baxter, Mt. Union; D. F. Berry, Noble; J. A. Carden, Winfield; G. H. Carter, Pekin; Miles Doran, Nichols; Robert Foster, Mediapolis; Charles Fye, Mt. Vernon; J. A. Frey, Noble; E. I. Hawk, Nugent; A. D. Hays, Wayland; C. E. Lemley, Wayne; E. L. McClurkin, Morning Sun; E. A. Miller, Packwood; W. H. Moorehead, Mediapolis; Earl Ogden, Martinsburg; Neri Ogden, Oskaloosa; W. S. Otto, Wapello; H. H. Riepe, Wapello; H. K. Smith, Olds; D. K. Unsicker, Wright; H. Van Dyke, Winfield; H. R. Williams, Yarmouth.

MEETING OF OFFICERS OF THE HAY ASSOCIATION.

The officers and board of directors of the National Hay Association held a meeting in Chicago, Dec. 13, for the purpose of conferring with the various railroad officials to protest against the changing of the classification for hay from 6th to 5th class, which is contemplated by the railroads, to go into effect January 1, 1900.

This would mean an additional tariff of about \$1 per ton on hay to the seaboard, and would entail a hardship and loss on western shippers of hay, who have made contracts for shipments based on the old rates.

The delegation consisted of President J. W. Sale, Bluffton, Ind.; Secretary F. F. Collins, Cincinnati, O.; George S. Bridge, Chicago, Chairman of Board of Directors; and Directors A. E. Clutter, Lima, O.; G. C. Warren, Saginaw, Mich., and J. A. Heath, Lenox, Mich.

In the evening eleven members of the Chicago Hay Receivers' Association gave those in attendance a dinner. President W. R. Mumford presided. After dinner, brief informal talks were made by President Sale, Secretary Collins, Directors Clutter, Warren, Heath, and Bridge Secretary R. H. Peterson of the local association, and H. H. Fisher.

The meeting was a pleasant and profitable one and those present adjourned, pledged to meet again next August at Baltimore, Md., at the seventh annual meeting of the National Hay Association.

The bank that financed the flour mill trust failed in New York Dec. 18, on account of its large holdings of trust shares.

A FAILURE.

BY MYRTLE DEAN CLARK.

CHAPTER 'II.

Now the time came when Margaret was much alone. John left the farm early, and returned late. How many times a day did Margaret go and look at the clock, to return disappointed that so many hours must elapse before she could run to meet her husband. When it was within an hour or so of the time, she oftentimes started to run in the direction of Calicut, then becoming conscious that, at any rate, she would nearly reach the town before John left it, she stopped to pluck the wild flowers, putting some in her hair, or thrusting them carelessly in her belt. But however Margaret arranged them, they were always artistic and seemed to cling to her as if they were a part of her, and knew that she appreciated and loved them.

Sometimes she let them run riot over her large leghorn hat and the effect of the tulle trimming and the vivid colors of the wild flowers would have driven a Parisian milliner into ecstasies.

John frequently found her thus bedecked, two or three miles from home, sitting on a stone waiting for him, or jumping from behind a tree laughing heartily at his surprise. Then she climbed into the wagon and they had a few minutes by themselves, each blessing the solitude of the road which led to the farm. She told him of the trivial happenings of the day, of how she had missed him, of how she was happy because she loved him so, even though it was "a wee bit lonesome."

And John, busy all day, meeting old friends and making new ones, did not think of how he had taken his girl-wife away from a home where she was petted and from the society of friends from whom she received much attention. He was tired when he came home and satisfied to find rest and be with her and thought of course that she too was content. (If he thought at all.)

But Margaret found each day quieter and duller than the last and longed for gayety, but tried to content herself with the thought that she was doing her duty by John; that some day they could move to Calicut and she could resume her former friendships. So she kept her secret to herself. She often asked John about business, but he made that common mistake of thinking that a woman should know nothing of financial dealings, not realizing that her keener intuitions often prove truer than his best business insight. So he answered her with a joke or appeared so bored that Margaret had not the courage to pursue the subject.

She resolved to employ all her time in learning to cook, sew and keep house for John, so that when they were by themselves in Calicut she would know just what to do and how to be a real helpmate to him.

She bought some blue madras cloth, when in Calicut, and started a shirt for John. His mother showed her how to cut it out and she resolved to make it all alone.

When John came home at night he seemed anxious to play cards. Margaret played with him, but was often worried because he became anxious and feverish over the game. Margaret would oftentimes let him win to please him, and again she would say, "John, I really believe you are angry whenever I beat and that spoils it all." Margare-

ret had always been accustomed to cards, while John's parents had absolutely forbidden him to play. Sometimes Margaret thought that John was trying to cheat, but she quickly banished that from her mind as being impossible.

Day by day, John grew more taciturn and self-centered. He petted Margaret less and less each day and was even cross with her when she questioned him. At first, Margaret thought he was ill and tried to prepare what he liked to eat to whet his appetite. He did not even notice the dainty dishes which she had learned to cook—dainty because of the way Margaret served them.

He was harassed by business troubles and the thought that he was beginning to live on credit. When night came, he wanted to drown his cares in cards, and so he and Margaret played, much to the sorrow of John's parents.

stitched and she must rip out the fancy work, then perhaps you would have a slight idea of what this blue shirt cost her.

Of course it did not look just as she would have liked. It was soiled from long handling; there was a bunch in the collar made in a last desperate effort to make it set right after she had been shown five times and John's mother had grown stern with her. It was polkadotted with drops of blood, but it was her work and for John, and she felt that he would be pleased with her. She put on a pretty gown that night and ran to meet John with the eagerness of a child who is about to bestow some sugar plums on one it loves.

She was anxious that there should be no discord between them and she felt that he was angry because she would not play poker with him the night be-



"When John finally found her in the woods, he found a different girl."

They went through the whole gamut of games—casino, euchre, cinch, bezique, whist, and finally John came home to teach Margaret to play poker. Margaret would not play with him and that night he left her alone and went to town. Margaret felt hurt, but said nothing.

The next day she worked diligently at the blue shirt with which she planned to surprise him. By night it would be completed. With what joy did she depict the scene when she should show it to John—her own handiwork, and for him. Perhaps you think it a trifle, but if you had seen the aching head bend over the task which was so new and difficult; had you seen the effort to keep the tears back when she pricked her finger and John's father laughingly told her to "squeeze hard and get the Dutch blood out" (a favorite expression of his); had you seen her dismay when John's mother smiled and told her that her stitches looked like a calf's tail and must all be taken out, or appreciated her disappointment when she found that John would be ashamed to have his shirt feather-

fore. She was walking along meditating and had not noticed his approach when he greeted her with: "Well, Sulks, are you going to play poker with me tonight?"

"No, John, not tonight, nor any night," she answered softly.

John was so piqued by her resistance and force of character that he answered angrily: "What are you good for, anyhow? You can't work or amuse a fellow, either. I call such folks nuisances."

Margaret bravely kept back the tears which struggled hard to be free.

When she reached home she brought John the blue shirt, and said simply: "I have been trying to be good for something, John." She turned abruptly away, afraid to trust herself to say aught else. John took the shirt and went into his room. Margaret was heartsick and very quiet, trying hard to suppress a choking sensation so that John might not know how deeply she felt hurt. Finally she heard him ejaculate: "This blamed shirt fits like a Mother Hubbard and buttons up wrong to boot."

Margaret quietly brought him another, gathered up the despised shirt in her arms, much as a mother rushes off with her insulted child, ran out doors burying her head in its folds.

Hours afterward, when John finally found her in the woods, he found a different girl; she seemed to him a woman, who looked at him with a different pair of eyes than those which had been wont to say whenever they met his "I love you." They were strange eyes which kept him at a distance. He found a woman who answered him with a voice which lacked the usual silvery laughing notes and the dulcet tones of happiness. It was a new, strange voice, and John was awed and could not tenderly and lovingly ask her forgiveness as he had planned, but merely said: "Come into the house, Margaret, you will catch cold in this damp air."

(To be continued.)

SUITS AND DECISIONS

By demanding an appraisal an insurance company admits its liability under its policy.

A mortgage taken for the purpose of defrauding creditors of the mortgagor is not merely voidable as to such creditors, but is void.

The Davenport Malt & Grain Co., of Davenport, Ia., has brought suit against Henry Woolner to recover \$4,238 alleged to be due for malt delivered at Peoria, Ill.

The W. T. Redmon Grain Co., of Kansas City, Mo., has brought suit against John Boyle and Pearl Mastick of Conway Springs, Kan., for \$256, an overdraft alleged to be due from Nov. 16, 1897, with 6 per cent interest.

W. T. Redmon has brought suit at Wichita, Kan., against J. W. Haun and J. M. Higgins, grain dealers, to recover \$6,000. To secure his claim Mr. Redmon attached several cars of wheat in the possession of the Santa Fe Railroad, one of which was replevined by Mr. Higgins.

Grain dealers are interested in a recent decision that under the war revenue law, judgment notes must bear a 25-cent stamp, in addition to the stamps required by the amount thereof. This is on account of the power of attorney, all instruments conferring such power being taxable.

A. Waller & Co., of Henderson, Ky., are defendants in a suit brought by the Mountain City Mill Co., of Chattanooga, Tenn. Defendant shipped a car of wheat billed at 64,200 pounds, and the mill company paid for this quantity. It is alleged the wheat on arrival weighed only half what it was billed at.

Judge McDonald, of the Superior Court at Colfax, Wash., will soon hear the suit involving the cancellation of mortgages on \$150,000 of property owned by Hamilton & Rourke, and held by Gibson & Kerr. The suit involves the entire warehouse system of Hamilton & Rourke, who are large dealers in grain. Gibson & Kerr are well known on the Coast as grain exporters.

A grain importer of Southampton, Eng., complains that the term t. q. (tale quale) affords the buyer no protection, as it is difficult to prove that corn was in bad condition when shipped. This firm received a cargo of corn from the Argentine, which was heated. Although the captain of the steamer testified that

the corn was heating when only two or three days out of port, the arbitrators at London gave an adverse decision.

The dispute between Messrs. Goodbody, of Liverpool, Eng., and Balfour, Williamson & Co., over the charter of the ship Vanduara, containing a cargo of wheat, has been decided in favor of the latter. The cargo was sold to be shipped "to any safe port in the United Kingdom." When the charter was presented, however, it contained the words "Manchester excepted." On this account Messrs. Goodbody refused to accept the cargo. The arbitrators found that Manchester was properly excepted, as the Vanduara could not safely pass under the Manchester bridge.

SEEDS.

Fire in the warehouse of the Cox Seed Co., New York City, Dec. 16, caused slight loss; fully insured.

The yield of clover seed in Iowa this season is reported by J. R. Sage, director of the crop service, at 4.3 bushels per acre; the total quantity at 780,000, and its value at \$834,000.

Free seed distribution costs the government \$15,000 a year. Since 1853 the government has expended \$3,476,802 in the distribution of seeds and the promulgation of seed statistics.

That clover seed or millet is one of the most important ingredients of factory-made raspberry jam, has been discovered by an Ohio pure food inspector. A barrel of jam requires one bushel of clover seed.

Toledo receipts of clover seed up to Dec. 22 have been: For the week, 2,255 bags; year ago, 1,940; this season, 98,358; last season, 50,821. Shipments, for the week, 2,540 bags; year ago, 745; this season, 70,539; last season, 18,575 bags. The supply of clover seed at Toledo is about 75,000 bags, against 90,000 last year, and 100,000 two years ago.

Clover seed can be handled at a profit only when bought right. Shippers who took it for granted that all the seed offered would grade prime paid too much for seed. Many dealers also paid No. 2 prices for Rejected, and lost money. Before getting in too deep, send samples to your commission man, and find out what the seed is worth.

The Department of Agriculture will begin its distribution of seeds a little earlier this year than last, shipping them south the beginning of January and perhaps sending a few shortly before the first. This year the seeds for distribution in all parts of the country will consist of 13,000,000 packets of vegetable seeds, 1,560,000 of flower seeds, besides field and lawn grass seeds.

In its review of the Chicago seed market for the week ending Dec. 23, the Daily Trade Bulletin says: A quiet week was passed in this market. In the timothy market there was some improvement, for demand was better—fully equal to supply. There also was a firmer and advanced clover market. Other lines were steady and very quiet. Interest in flax fell off materially. Prices did not change much, and the market was so quiet as to be dull.

The wheat crop of New South Wales is estimated at 13,500,000 bushels; against 10,500,000 bushels last year. The quality is good.

COBS.

Henry M. Friend is carrying on an expert inquiry with reference to grain gambling in the United States, for the Industrial Commission.

Flaxseed receipts at six principal markets during four months ending Dec. 1, were 15,078,000 bushels; against 12,160,000 bushels for the corresponding period of 1898.

The British government is discussing the feasibility of building national granaries and storing vast quantities of wheat against the emergencies of war or famine.

Work on the Great Northern Elevator at West Superior, Wis., has been stopped and will not be resumed until next spring. Material cannot be had for the iron work.

The corn crop of Mexico during 1897, the last year for which official figures are available, was 121,892,000 bushels; against 72,264,000 in 1896; and 71,905,000 in 1895. Corn in Mexico is the food of the people.

An ear of corn with 30 rows and 1,153 grains was exhibited recently at Kewanee, Ind. It shelled out 1½ pints. Another novelty in corn at the same place was two stalks, each with an ear, from one root; and still another was two perfect ears on the same stem.

The wheat crop of Victoria, Australia, is estimated by the Melbourne Argus at 9.37 bushels per acre, and the export surplus 13,000,000 bushels. In 1898 the produce of wheat in Victoria was 6.38 bushels; in 1897, 4.49; in 1896, 4.01; in 1895, 8.38; and in 1894, 10.38 bushels.

Chicago Board of Trade wishes to suppress bucket shops. That's right. They help to depress grain prices. If every bull who has bought with them had placed his orders upon the regular Boards of Trade, wheat would now be several cents higher. They bet you will lose. They try to force the market against you. They trade entirely upon the Chicago Board of Trade and New York Stock Exchange quotations. Devise some scheme to prevent them from securing them, and their occupation would be gone. State laws are not very effective, especially in Chicago. National laws command more respect, because they are better enforced.—C. A. King & Co.

From the annual report of the Secretary of Agriculture it appears that the crop-reporting branch of the Division of Statistics has 41 salaried state statistical agents, with 8,730 correspondents, upon whose reports their monthly statements were mainly based; 2,627 county correspondents, with 7,881 aids and 36,426 township correspondents reporting each for his own neighborhood. From this large body of persons—selected with great care, not only as to their geographic distribution, but also as to the qualifications for the performance of the duties required of them—reports have been received monthly, and at the close of the calendar year a select body of farmers, numbering about 90,000, reported upon the crops of their own individual farms. Of the regular periodical report of the Division there has been printed a total of 1,621,700 copies. These reports cover that general work of the Division which is continuous in its operation and which has constituted for a generation or more the only source of information available to the farmer that has been comprehensive, prompt, and unbiased.

SCALE BEAMS.

The average grain man gives very little, if any, consideration to the different features of the beam of his scale. When buying the sentiment which oftentimes prevails is, "get the cheapest." Reliability, length of time scale will give good service, and the facility with which the weight of grain may be determined are seldom considered.

The beam is not the only part of the scale which merits careful consideration. The length of platform has much to do with the value of the scale, of which fact, no doubt, the readers of The Journal were convinced by the many letters published during recent

weighing, had its beams extending to the left of upright. Most of his purchases were of grain. The beam on his wagon scale extended to the right. There he read the figures on the left hand side of sliding poise. As he did much more weighing on this scale than he did on his wool scale, he became accustomed to reading the figures on the left hand side of the sliding poise. This made so much difference that in a couple of years he found that he was gradually losing money, despite the fact that he had been confident he was gaining. He made a thorough investigation of his books and finally called in an expert bookkeeper to help him convince himself that he had been robbed of

devices which materially reduce the opportunities for error have not become more popular with all grain weighers. It is true that many imperfect devices have been put on the market and discarded wherever introduced, yet there are others whose efficiency has been proven by experience.

The detached iron weights so convenient for cracking nuts, driving nails and various other duties generally allotted to the hammer, seem to be disappearing. The natural wear of the iron by attrition in sliding the weights over one another, oftentimes has made weighing with them incorrect. The brass or steel poises which are attached to beam overcome the objections to detached

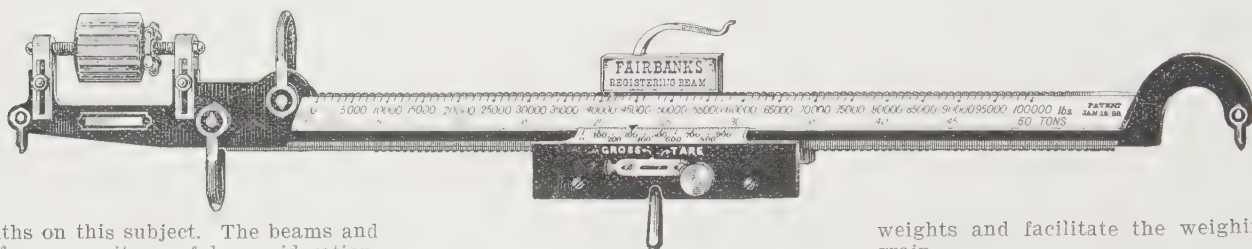


Fig. 1.

months on this subject. The beams and platforms merit careful consideration, but not more careful consideration than the design, and the material used in construction. Defective weight, however, does not always prove the scale is defective; in fact, the weighman is far more to blame for incorrect weights than the scale. There are opportunities for error in weighing, error in reading weight, error in recording weight, and error in determining net weight; and again the weighman has an opportunity for making error in reducing pounds to bushels. It is very often that errors occur in this last transaction. It is natural, however, that error from all these sources should be attributed to the scales.

It is essential that the grain buyer keep his scale in first-class condition. Inspection by a scale expert once in six

months on this subject. The beams and platforms merit careful consideration, but not more careful consideration than the design, and the material used in construction. Defective weight, however, does not always prove the scale is defective; in fact, the weighman is far more to blame for incorrect weights than the scale. There are opportunities for error in weighing, error in reading weight, error in recording weight, and error in determining net weight; and again the weighman has an opportunity for making error in reducing pounds to bushels. It is very often that errors occur in this last transaction. It is natural, however, that error from all these sources should be attributed to the scales.

weights and facilitate the weighing of grain.

Recording beams have been on the market for some years. Some of the early devices were so very impracticable as to be discarded before many of the members of the grain trade had learned of their existence. Only one of the early devices has lived. It and one other recording device are illustrated herewith. Both are simple; both seem practicable.

There is so much trouble arising from shortages in grain shipments, that it would seem to be the part of wisdom for grain shippers and terminal elevator men to adopt the recording device which seems best suited to their especial needs. The nearer weighing of grain is reduced to a mechanical operation the fewer opportunities will there be for error in the work. The scale

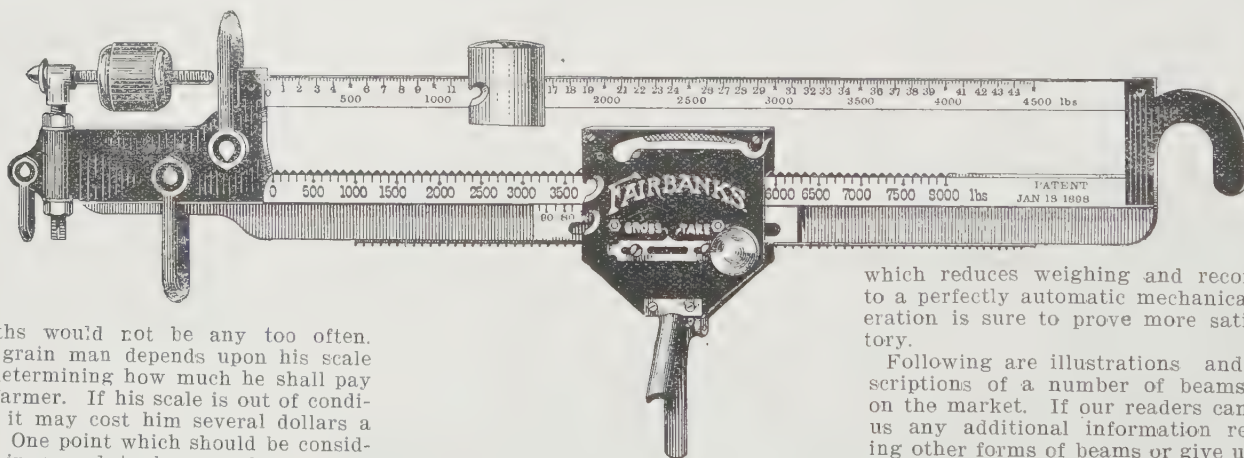


Fig. 2.

months would not be any too often. The grain man depends upon his scale for determining how much he shall pay the farmer. If his scale is out of condition, it may cost him several dollars a day. One point which should be considered in regard to beams when buying scales is that the markings should be so clear as to enable the weighman readily to perceive the correct weight of load.

It is not often that a grain man is called upon to weigh upon two pairs of scales having beams pointing in opposite directions, yet this very thing occurred in a Michigan town not a great while ago. The grain man was also a dealer in wool. He bought wool from the farmers and weighed it in over a platform scale which had a sliding poise on bar. This scale, unlike the scale over which he did most of his

him several thousand dollars, which, of course, was a clear gain to the farmers, none of whom were willing to return the money erroneously paid them.

The experience of the Michigan dealer has not been repeated often by grain dealers in other parts of the country, yet many of them continue to use beams which increase the opportunities for error in weighing, as well as in reading.

The trouble of shortages in grain shipments has agitated the trade for so long a time that it seems somewhat difficult to understand why recording

which reduces weighing and recording to a perfectly automatic mechanical operation is sure to prove more satisfactory.

Following are illustrations and descriptions of a number of beams now on the market. If our readers can give us any additional information regarding other forms of beams or give us reasons why they like one form better than another, we would be pleased to hear from them.

FAIRBANKS BEAMS.

The cut of the Fairbanks beam, Fig. 1, shows their railroad track scale beam with the registering device. This beam is constructed on the well known principle of the full capacity railroad pattern beam. It has a beveled face, graduated and figured, so that the weights may be read in the usual manner. The under edge of the beam is provided with a series of type figures corresponding with the weight graduations. The poise

is provided with an internal mechanism, so that when the load has been balanced upon the scale in the usual manner, a card may be inserted in the slot of the under poise and by moving the handle attached to the poise an impression is made on the card which shows the gross weight of the load. There is a semi-automatic mechanism which causes the tare weight to be printed on the same card under the gross weight when the empty car is returned, so the net weight is easily determined.

By Fig. 2 is shown a double beam especially adapted for use in weighing wagon loads and live stock. This beam is similar in construction and operation to the first beam, except that it is provided with a graduated tare beam, so that if several loads are to be weighed, using the same wagon, the upper poise can be set at the figure representing the uniform or constant tare, the net weight can be obtained and recorded on the ticket without reweighing wagon. Both of the above beams dispense with the use of the loose weights.



Fig. 3 shows the hopper scale pattern beam, which is especially adapted for grain hopper scales. This beam is constructed of steel, with a brass face, has great rigidity, and can be used equally well for wagon or other scales. The under edge of the beam is provided with type corresponding to the graduations on the beam, which is graduated to 1,000 pounds. It is also provided with a graduated counterpoise, which has type on edge corresponding with the figures on face. Weights are employed as with ordinary scales. When a balance is obtained a ticket is inserted in the slot of the poise and of the counterpoise slides. By moving the handles on each slide after card is inserted, figures are indented on card's surface showing the weights indicated by beam and by counterpoise weights. For example, if the amount weighed is 8,550 pounds, as is shown by cut, the figure 8 is impressed on card by the counterpoise attachment and 550 by the beam attachment, thus making a total of 8,550.

The Fairbanks Compound Stock Scale Beam is shown by Fig. 4. It is used on scales ranging from 4 to 15 tons' capacity. The upper bar of this beam is graduated for tons in 200-pound notches. The left-hand end of the lower beam is graduated in 2-pound spaces for 200 pounds, and is used to take the intermediate weight between the notches on the upper bar. The rest of the lower beam is graduated for 2,000 pounds and is used as the tare beam. By having this style beam on scales the users are protected against error from counting weights necessary when a counterpoise is used. The ordinary 2,000-pound weight belonging to the 4-ton scale weighs exactly 4 pounds. These weights

are not infrequently used for driving nails, and with this usage and ordinary wear it loses weight. Even if it loses but one-fifth of an ounce of its weight (this one-fifth ounce represents about six pounds on the platform of the scale, or on an average load of three tons), this discrepancy would be over eighteen pounds on each draft against the buyer. So it is to the advantage of scale owners to guard well their loose weights, or equip their scales with a compound beam.

STANDARD BEAMS.

The Standard Scale & Supply Co. has sent us a number of illustrations of beams that are used on weighing machines designed particularly for grain dealers and millers.

Figure 9 shows the ordinary, single beam having a graduation from zero to 500 lbs., or 1,000 lbs., stamped on its sides. Weights of heavier loads are ascertained by additional weights placed at the end of the beam. The sum of these weights and the indicated position of poise establishes the actual

weight of the load, inclusive of wagon. The net weight of goods can be obtained by subtracting the weight of the vehicle from the gross weight of the load. For cattle weighing this beam is preferable, and for weighing on a farm it meets every requirement.

Figure 10 shows a more desirable double beam, having on the upper bar a graduation to 1,000 lbs., on lower bar 2,000 lbs. This beam offers the advantage of setting the slide poise on the lower beam to balance the wagon and of computing the net weight directly from the sum of weights at the end of beam and the position of the upper poise. Grain and coal wagons can be quickly weighed in succession to net pounds. The lower poise can also be used to balance quickly the scale, owing to variations in weight of the platform, on account of the accumulation of dirt on same, or of increased weight after a heavy rain, etc.

Figure 11 shows a triple beam. The advantage of it lies in the arrangement of the beams, having on its sides the full capacity of scale so distributed that the gross and net weight can be obtained without any mental calculations. Set the middle poise to the weight of wagon, move the lower poise to the nearest 1,000 mark and the balance on the upper poise. As the lower beam is only graduated to every 1,000 lbs., and the upper from zero to 1,000 lbs., the reading is manifestly certain, avoiding the addition of weights at the end of beam, and the annoyance of loose weights, which are liable to become lighter from rust or by attrition.

Figure 12 shows the compound beam, an arrangement with the same advantages as the triple beam. The lower

beam has a graduation to cover the tare of the heaviest wagons, the main or upper beam has the capacity of scale stamped on its side to obtain such loads which may be desired to weigh gross. This beam is very substantially gotten up for heavy and continuous duty as a time saver and quick weigher, with no possibility of making errors in calculation.

Figure 13 shows the ordinary railroad scale beam having full capacity indicated on beam. There is no provision to obtain the net weight by setting auxiliary poises from the fact that railroad agents go by the marked weight given on the car when computing freight charges. Manufacturers, shippers and receivers find it to their advantage to weigh the loaded car, as well as the empty car, to ascertain the net load. In weighing, the heavy poise, which works smoothly and easily on rollers, is moved by pressing down on the latch or trigger. The approximate weight of the load is quickly ascertained, then the latch is released and the poise drops in one of the upper notches of the beam.



Fig. 3.

The odd pounds are taken on a small beam attached to the bottom face of this large poise.

Figure 14 shows the double railroad scale beam with tare beam, the lower beam having a graduation of 35,000 lbs., which covers the heaviest empty freight cars at present. Its use and manner of operation is the same as on hay scales.

Figure 15 shows the triple beam for railroad track scales, used at large coal mines for weighing the loaded railroad car to net pounds, setting the tare for empty car and crediting the miner for the amount contained in his pit car. The scales connected with this beam have always a platform twice as long as the regular cars, so when one end is loaded the car is gradually moved farther until complete, and then weighed. The mine operator can with such a beam pay his men correctly and bill out his coal by reading the weight on the beam.

Figure 16 shows the gridiron beam, also for coal weighing, and inasmuch as these separate beams are a check when the position of poises is secured after a pit car has been emptied, it enables the weighmaster to compare the sum of all grid beams with the main beam,

when the car is completely loaded. A coal mine with small pit cars will need a beam with more rigid grid beams than another mine using larger pit cars. Track loaders could use this in determining weight of loads of grain in loading car.

Figure 17 shows a recording beam to indicate the number of pounds at which the load has been weighed. A ticket is punctured at the proper figures, and so forms a legal proof of the transaction of weighing. It will be clear to any investigator that this beam gives to the user

larger cities, avoiding the annoyance of loose weights, enabling the weigher to get the weight of loads more quickly, and with less liability of mistakes. Dormant flour, portable scales and grain scales, are usually provided with single beams. Dormant warehouse scales with double beams, the upper beam indicating 100 lbs., and the lower beam up to 200 lbs., the lower poise being set at weight of truck used. The sum of the loose weights is indicated by a graduated counter poise hung at the end of the beam, and this total,

distances. These extensions can be applied to any make of scale. Where it is desired to get an improved form of beam, an exchange can be made, and the new beam "sealed" to the old one.

AUSTIN'S IMPROVED WEIGH BEAMS.

Austin's Patent Weigh Beam, double style, with goose neck stands, as manufactured by the United States Scale Works, Terre Haute, Ind., is shown by Fig. 5. By using these beams the need of loose weights is done away with,

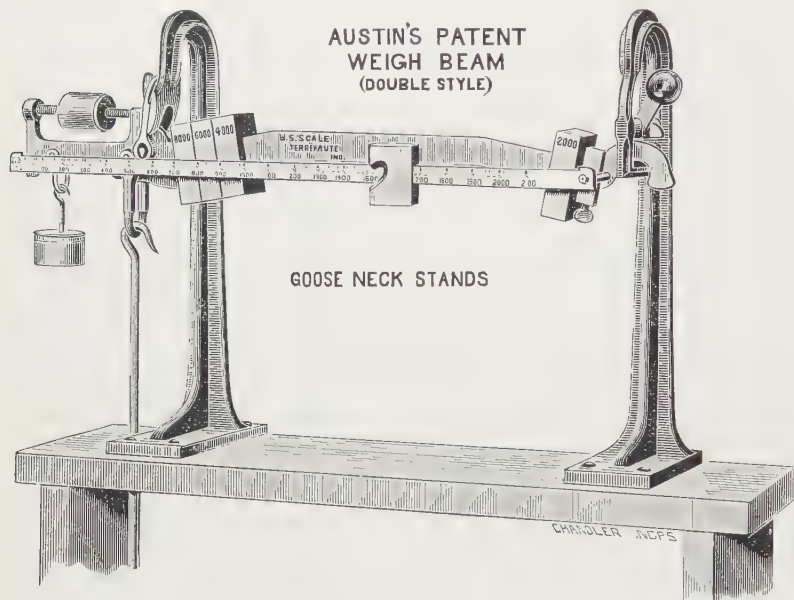


Fig. 5.

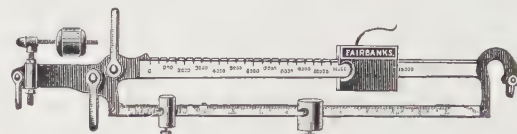


Fig. 4.

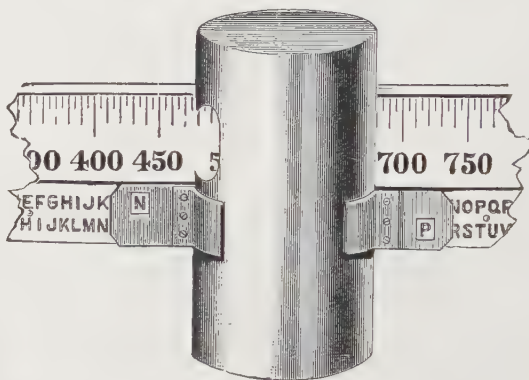


Fig. 7. Demuth Check Beam.

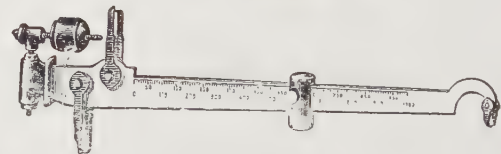


Fig. 9. Single Beam.

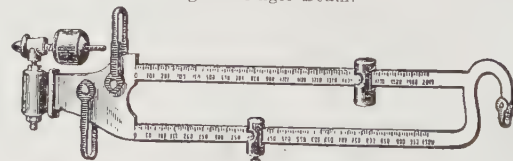


Fig. 10. Double Beam.

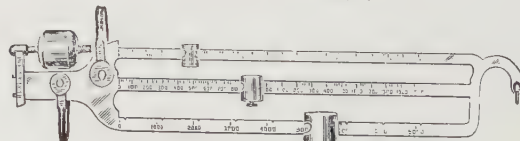


Fig. 11. Triple Beam.

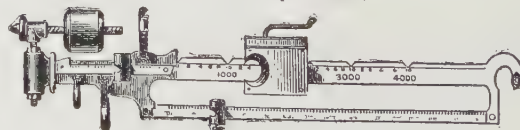


Fig. 12. Compound Beam.

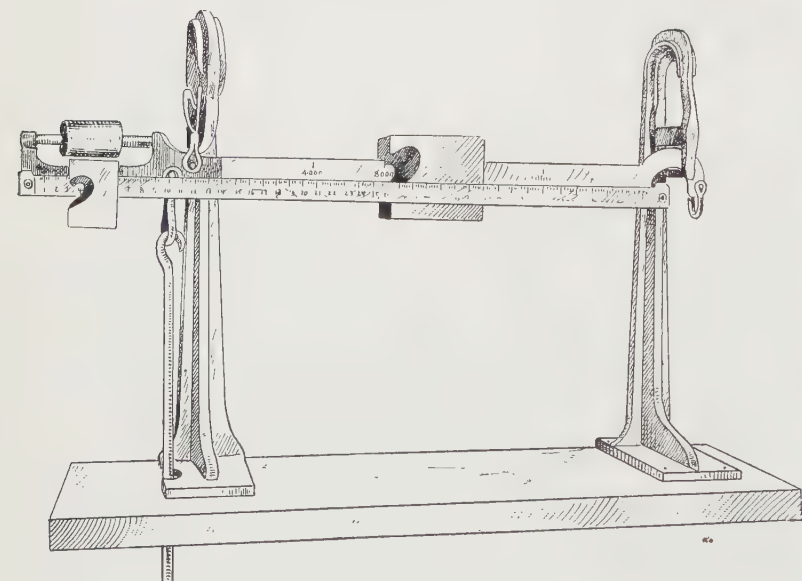


Fig. 6.

something to rely upon when disputes arise. In grain elevators, stock yards and freight stations, where much and important weighing is done, this beam will be appreciated in preventing those errors due to misreading and wrong writing of figures. The ticket is clearly printed and the puncturing of ticket is always correct.

All modern beams are of the sliding poise variety. Tare beam poises are provided with set screws. The beam illustrated in Fig. 3 is also much used on wagon scales, especially in the

and the position of poise on upper beam, gives the net weight of the load.

Hopper scale beams are of the single beam variety, and are sometimes provided with "wheat weights" or "corn weights," although, as a rule, the ordinary weights are to be preferred. Beams can be hung in a beam box, the latter provided with door, lock and key, or hung from ornamental beam fixtures, where beam is carried inside the building—in the latter case, it is often necessary to provide extension levers, for carrying the beam unusual

hence there are none to be lost, stolen or tampered with. The back or main beam carries the movable weights or slides, which, when not in use, rest on the slope, which keeps them close together and preserves the balance of the beam, when jarred by passing loads. These slides are all of the same weight and each being on a bottom balance at the start, and moving over the same distance, will give the same result. Each of the four slides on the main beam count 2,000 pounds when moved to the right against the collar. The slides

being plainly marked, there is no time lost in finding the right notch, for moving them quickly as far to the right as they will go. As a weigher soon learns to guess within one ton of the weight of a wagon load of wheat or corn, it will be seen that a beam of this kind will save much time, and also reduce the possibility of error. The front beam registers 2,000 pounds in 5-pound notches. It is constructed for rapid, accurate work, is largely used on wagon scales and is simpler and quicker than the buggy poise system.

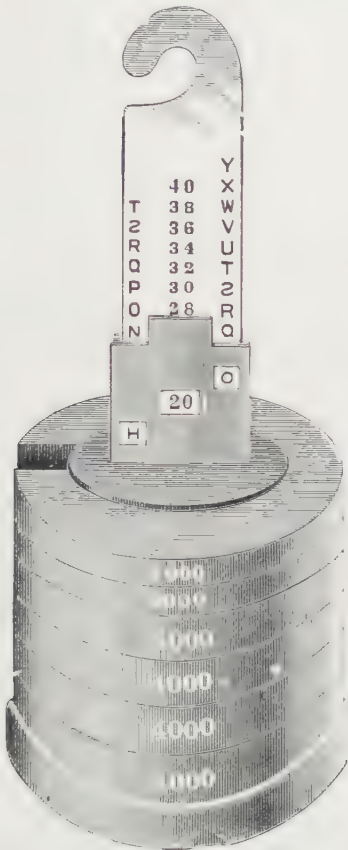


Fig. 8. DeMuth Check Beam.

USE NO LOOSE WEIGHTS; NOTCHES ON UNDER SIDE.

W. H. Caldwell, sales agent of the Government Standard Scale Works, Terre Haute, Ind., writes: We make varied form beams to suit special demands. Figure 6 shows double graduated beam with sliding poise. The bar of beam with small poise is graduated from 0 to 1,000 pounds; the large one from 500 to any amount, depending on capacity of scales. This beam is very satisfactory for wagon, track and hopper scales. For coal mine scales we use three bars, one of which is used to take weight of mine cars, the other two to get exact weight of cars and contents.

We do not use any loose weights in connection with our scale beams. The large poise on all our scales is mounted on a single roller near its center, this roller running on top of beam. There are notches on the under side of beam to correspond with figures on front of same.

When weighing, the operator places hand on the large poise so as to make it balance on the roller. This releases the dog from the notch; it is on the under side of the beam and allows it to move easily to the desired point, and

as the operator releases hold on poise the heavy end drops and causes dog to catch in the notch on under side of beam. With a large sliding poise it is essential that it stop exactly in center of these notches, as variation of a small fraction causes incorrect weight. By having the notches on under side of beam, grains of dirt never get in them, and the dog can always come to center in them, thus insuring accuracy.

DEMUTH'S DOUBLE ENTRY SYSTEM FOR RECORDING WEIGHTS.

The principal feature of Demuth's system is entries in a tally book of the weights in figures and letters, the figures being read from the beam at one point, and two letters which stand for figures at two other points. Thus, if 480 on the beam should read 450, the letters N. P. would show that the poise

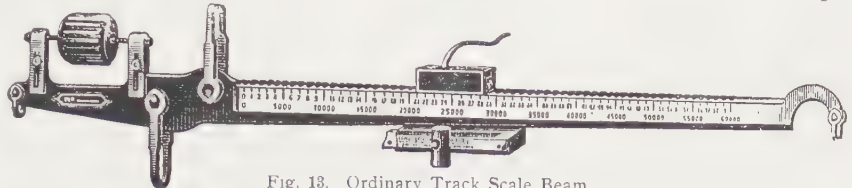


Fig. 13. Ordinary Track Scale Beam.

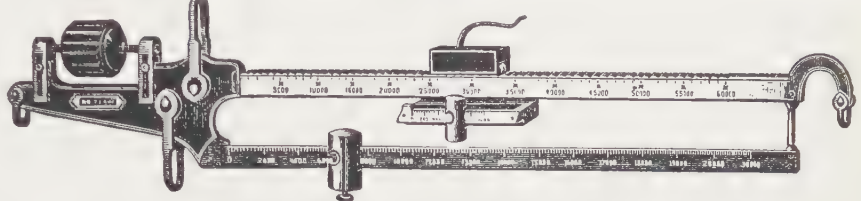


Fig. 14. Double Railroad Scale Beam.

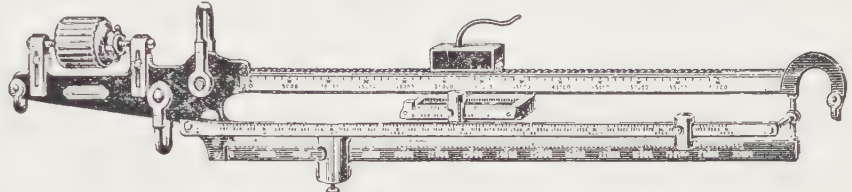


Fig. 15. Triple Beam for Track Scales.

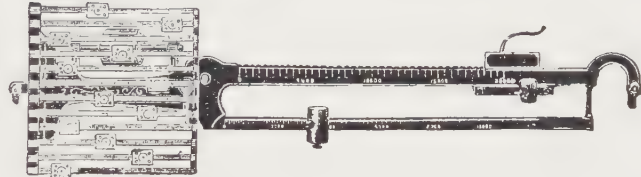


Fig. 16. Gridiron Beam.

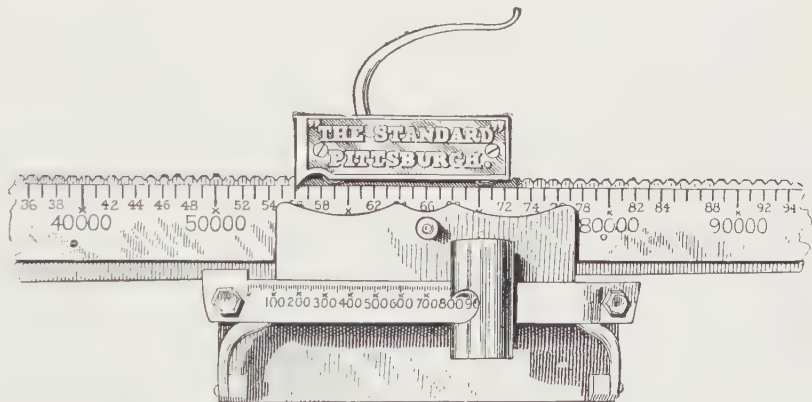


Fig. 17. Recording Beam.

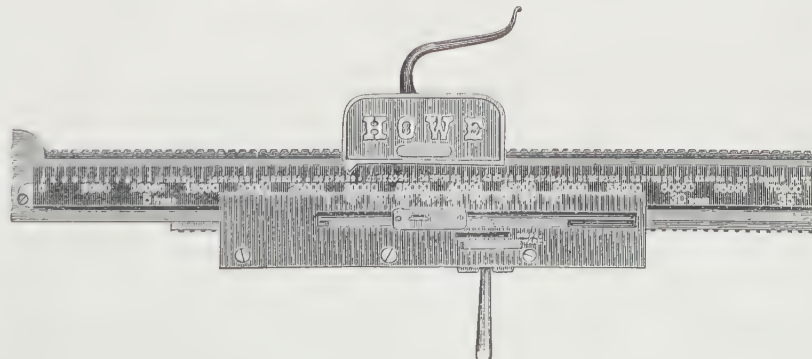


Fig. 18. Beam with Recording Poise.

stood at 480. And so if the figures should be recorded 480 but the letters M. P., the fact that 480 calls for N. P. instead of M. P. would be plain. For every possible weight there is a particular combination of letters, which are tabulated with their corresponding weights for ready reference. Thus the tally book contains a double entry of the reading of the beam, and makes it possible to balance the book. In this way the elevator receipt may be balanced with the original figures indicated by the scale, instead of the weighman's unproved copy of the original figures.

A feature of this system is the complete entry in the tally book, both entries being made from the scale. There is no machinery, stamping, printing, or automatic action connected with this system—the beam enables the weighman to use the same principle which the ac-

countant uses, to-wit: More entries than one of the same transaction, in different ways and in different places.

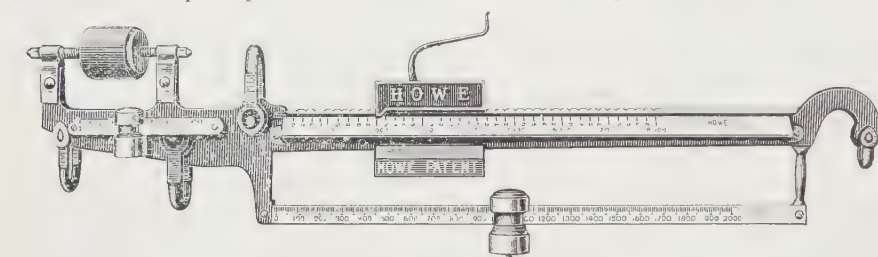


Fig. 19. Custom House Beam.

countant uses, to-wit: More entries than one of the same transaction, in different ways and in different places.

The bar and counterpoise of the De-muth beam are shown by rigs. 7 and 8.

THE HOWE BEAMS.

The illustration Fig. 18 shows a track scale beam with the Howe Recording Poise, which is used for obtaining a printed record of the weight. The weighing is done with an ordinary beam, by moving the main poise to the nearest notch and then moving the small slide until an exact balance is obtained, when the weight may be read as usual. In order to obtain a printed record of the weight it is only necessary to insert a ticket in the slot in the face of the poise and pull the handle underneath. This movement forces the card into contact with sharp faced hard bronzed type, arranged to correspond with the graduation on the beam. It makes a very distinct impression without the use of ink. The face of the poise has two slots, used especially to take gross and ton weights and their relative position is such that the weights will be printed one above the other for convenience in subtracting. By the use of a recording beam duplicate weights can be taken, which may be of service in settling disputes as to weights at some future time.

By Fig. 19 is shown the Howe United States Custom House Beam for use on stock and farm scales. The main feature of this beam is that it does away with loose weights, as will be seen, the main or top bar has a brass bevel face that is graduated to 200 pound indications and each one notched, while the lower or tare beam has sufficient capacity to balance a wagon or a stock rack, varying according to the capacity of the scale and length of the platform and is graduated to 5 pound notches. The small or auxiliary beam indicates 200 pounds, by $2\frac{1}{2}$ pound notches, and is used to take the intermediate weight between the notches on the upper bar.

NEW CALENDAR STAND.

A good daily memorandum calendar is a great help to the office man. One of the best that has come to our notice is represented in the engraving herewith. The Standard Calendar Stand is entirely different from others, takes up very little desk space, has rubber feet to prevent scratching the desk, and is finely finished. It is an ornament to any desk. The memorandum pad is $2\frac{1}{2} \times 4\frac{1}{2}$, and gives a large amount of writing space. The base, complete, is $4\frac{1}{2} \times 5\frac{1}{2}$. The past

and future months are shown on each memorandum leaf; and the twelve months of the current year are always in view. Expired leaves cannot be blown, knocked back or disturbed in any



Calendar Stand for 1900.

way. The calendar can be obtained of S. D. Childs & Co., of Chicago, who furnish it in three styles of finish, black japan, nickel, and oxidized.

NEBRASKA CORN CROP.

A. H. Bewsher, secretary of the Nebraska Grain Dealers' Association, has issued his crop service report No. 4, dated Dec. 15, from which we find that 400 replies were received from 675 blanks issued, bearing upon the corn crop of 1899. Summing up, we have a total yield of 175,816,641 bushels, from an area of 6,436,918 acres devoted to corn. This acreage, obtained from country clerks and other reliable sources, is verified by replies received, setting forth the increased acreage as about 7 per cent greater than that of last year, when it was 6,077,021 acres. The average yield per acre for the state is 27.3 bushels.

There has been practically no injury considering the state as a whole since maturity, 89 per cent being reported as good enough to grade No. 3 or better.

This is about 11 per cent better than the quality of last year's crop. It is estimated that it will require 43 per cent, or 75,580,267 bushels to satisfy the home feeding demand. This is about $5\frac{1}{4}$ million bushels less than it was estimated would be required for last year's feeding. Reserves carried over by both farmers and speculators amount to about 7.6 per cent of last crop, or 9,953,852 bushels, a very small percentage indeed when compared to the amount carried in the 1898 crop from previous years. It is generally believed that speculators will crib much less this year than last. In three divisions, the First, Fourth, and Fifth, the farmers will crib more. In the others it is thought they will crib less.

Replies indicate that the movement generally throughout the state will be slow, many causes being given for this, principally among them the fact that the farmers expect higher prices later in the season, the anticipation of a brisk feeder demand during the winter, and the short crop.

Deducting reserves carried from the 1898 crop, 9,953,851 bushels, we have 185,770,493 bushels. Deducting also 75,580,267 bushels required for feed, and 17,581,664 bushels for other purposes, leaves 92,608,562 bushels available for shipment across county lines.

GRAIN CARRIERS.

The Missouri & Iowa Southern proposes to build from Sedalia to Miami, Mo.

The shortage of cars in New England has been very serious during the past four months.

It is proposed to extend the Kansas Midland Railroad from Wichita to South McAlester, I. T.

The Atkinson & Northern Railroad has graded 22 miles of its line from Atkinson toward Perry, Neb.

Bills to subsidize steamer lines have been introduced in Congress by Senator Hanna and Representative Payne.

The Tennessee Central Railway has 3,600 men at work on the 60 miles of road between Emory Gap and Monterey.

The Colorado, Texas & Mexico Railway Co. is about to begin work on its proposed line north and south of Abilene, Tex.

New freight terminals at Kansas City, costing \$250,000, have been completed by the Chicago, Burlington & Quincy Railroad Company.

The Albert Lea & Southern is to be built from Albert Lea to Lyle, Minn., to connect the Illinois Central with the Minneapolis & St. Louis.

The Illinois Central is said to be surveying a line from Omaha to Sioux City, 90 miles, connecting at Loveland, Ia., with the Fort Dodge & Omaha.

The International Warehouse & Transit Co. has been incorporated in South Dakota by St. Louis men to transport grain by river from the Northwest.

Resolutions favoring the construction of a ship canal across Illinois were unanimously adopted at the recent meeting of the state grange at Joliet.

The steel steamer Arthur Orr, with 70,000 bushels of corn shipped by Armour & Co., from Chicago to Depot Harbor, went on the rocks at the entrance to Georgian Bay, Dec. 15, and was disabled. Tugs succeeded in towing the steamer to a safe harbor.

THE SUPPLY TRADE

A handsome calendar is being sent out by the Union Iron Works, Decatur, Ill.

H. W. Caldwell & Son Co., of Chicago, have completed their new shops and are now moving their machinery from the old plant.

We are indebted to the Joseph Dixon Crucible Co., of Jersey City, N. J., for a box of Dixon's Superior Pencils, "With the Compliments of the Season."

Col. George M. Moulton, of the firm of Moulton-Starrett Co., Chicago, Ill., has been appointed to the office of inspector general of the Illinois National Guard, with the rank of Colonel.

John H. Porter, who has charge of the advertising department of the Barnard & Leas Mfg. Co., Moline, Ill., was in Chicago last week, and reports that the Barnard & Leas Co. has never been so busy before at this season of the year.

One of the most acceptable Christmas souvenirs sent to the trade this year was the nickel plated pocket match safe sent to the friends and patrons of J. F. Zahm & Co., Toledo. Recipients will have an extra cause to scratch for Zahm.

The Grain Shippers' Mutual Fire Insurance Association of Ida Grove, Ia., is sending to its friends and customers a very neat calendar for 1900. The one received has a reproduction of the famous painting called "Sight," by H. Spirling.

The Weller Mfg. Co., Chicago, has appointed Crofut, McAyral & Co., 49 First St., Portland, Ore., as agents for the northwestern territory. They will carry in stock a complete line of such goods as is practicable to carry, of the Weller make.

John S. Metcalf & Co., of Chicago, have filed articles of incorporation for \$50,000 under the firm name of The John S. Metcalf Co., to do a general manufacturing, engineering and contracting business. The incorporators are John S. Metcalf, F. G. Adamson and T. K. Webster.

The Chase Elevator Co., 12 Pacific Av., Chicago, has issued a circular describing the Chase-Kaestner Combined Oat Cleaning and Clipping Machine and pointing out its advantages. It also gives a few facts about clipping oats, telling how to clip for home and export trade.

One of the neatest and most useful calendars which has come to our notice is a desk calendar encased in leather. It is being sent out with the wishes for a very prosperous New Year by Ware & Leland, Chicago. On the back are printed two small complete calendars for the years 1900 and 1901.

The Marseilles Mfg. Co., Marseilles, Ill., has made some changes among its branch house managers, as the result of the death of S. A. Cram, the former manager of the Council Bluffs house. W. S. Case, its Kansas City manager, has charge at Council Bluffs, C. H. Booth at Kansas City, and C. N. Ward at Peoria.

The S. Howes Co., Silver Creek, N. Y., has appointed W. A. Walker its western agent, with headquarters at Chicago. Mr. Walker is a member of the Chicago Board of Trade, has been in the grain business for a number of years, and enjoys a large acquaintance

with the grain trade, not only in Chicago, but also through the West.

The N. P. Bowsher Co., of South Bend, Ind., is sending to its friends and customers a neat condensed booklet that contains much information of practical value to elevator men. The outside rear cover bears a handsome colored engraving showing the most popular styles of the Bowsher mills. The booklets are sent free on application.

Reynolds Bros., of Toledo, O., are sending out their usual artistic calendar. This year they show the periods of popular fashions during the 19th century. First is a girl of 1900, then of 1880, 1860, 1840, 1815, and 1800. The pictures adorning this calendar are direct reproductions from valuable paintings by one of America's best artists.

Rosenbaum Bros. & Co., live stock commission merchants, Chicago, are supplying their friends and customers with a colored wall map, 20x22 inches, of the world. Besides the firm's announcement, it gives the civil divisions of the world, arranged by continents, giving each country, its area in square miles, the population, government, chief executive, the capital, its population and the annual exports and imports.

PATENTS GRANTED

John Titus, of Oyster Bay, N. Y., and William Titus, of North Hempstead, N. Y., have been granted letters patent No. 639,003 on a conveyor apparatus.

William S. Osborne, of St. Louis, Mo., has been granted letters patent No. 638,983 on a dust collector and assigned one-half to Elwin C. Bryant, of same place.

Francis H. Richards, of Hartford, Conn., has been granted letters patent No. 638,672 on an automatic weighing machine and assigned same, by mesne assignments, to the New England Automatic Weighing Machine Co., of Boston, Mass.

D. A. Robinson, of Minneapolis, Minn., and Max Toltz, of St. Paul, Minn., have been granted letters patent No. 638,280 (see cut) on a grain bin. The principal feature for which this patent has been issued is the hoppers bottom, the upright posts and circular girders supporting it.

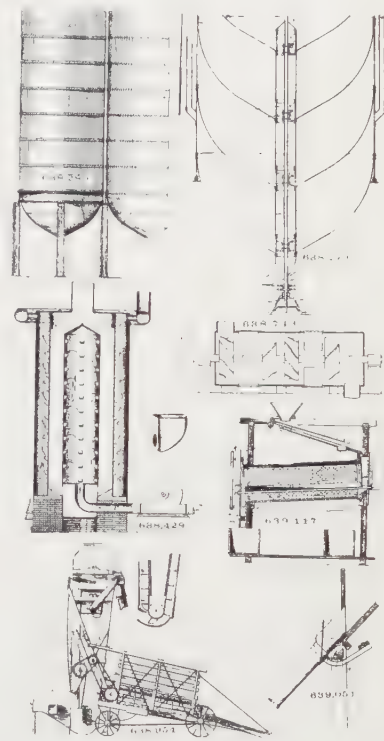
H. H. Carr, of Chicago, Ill., has been granted letters patent No. 638,954 (see cut) on a portable elevator. This comprises a wheeled truck having a motive power mechanism, an elevator boot or casing foldable on the truck. It is to be used for loading grain direct from farmers' wagons to cars.

George H. Hutchinson, of Wilkesburg, Pa., has been granted letters patent No. 639,051 (see cut) on a chute for bins. The principal feature of this chute is that it has a swinging extension, which acts as a shut-off door when the extension is raised at right angles to the main chute, thus preventing the flow of any material.

Frederick A. McLellan, of Ft. Worth, Tex., has been granted letters patent No. 638,743 (see cut) on a grain scourer. This consists of a case that has an inlet and an outlet opening. A shaft running through the case, containing conveying beaters, with a whirl of blades and retarding beaters with a whirl of blades. The retarding device is arranged with a

space between it and the conveying device sufficient to allow an accumulation of a mass of grain which is acted on by the beaters.

Albert W. Walker, of Salem, N. C., has been granted letters patent No. 639,117 (see cut) on a grain separator and cleaner. Eliza C. Walker, administratrix of said Albert W. Walker, deceased, has assigned same to Camillus G. Lanier, of Winston, N. C. This machine consists of a shaking shoe carrying a plurality of screens of different mesh and having lateral spouts leading therefrom. A rotatable hollow cylinder having internal pockets or cells, with a shoe running through interior of cylinder.



William Tweedale and William L. Harvey, of Chicago, Ill., have been granted letters patent No. 638,429 (see cut, on an apparatus for drying grain. This drier consists principally of a vertical outer wall or cylinder and a vertical inner wall or cylinder having a closed top. These walls are made of thin metal. The inner and intermediate walls are provided with air ports, with guards to prevent the passage of grain through the ports. There is a means for forcing hot air through the circular wall of grain. The grain moves continuously through the drier at any speed desired by the operator.

D. A. Robinson, of Minneapolis, Minn., has been granted letters patent No. 638,279 (see cut) on the subdivision of grain bins. This is the combination of a cylindrical wall extending from top to bottom of the bin and divided into separate compartments by one or more suspended divisions arranged transversely to the axis of the cylindrical body of the bin. There is a tube or spout passing through the bins which has openings in each compartment, a slide, valve or gate for each opening, a rod through the tube, and is provided with arms to engage the gates or valves for opening and closing one or more of them without moving the other.

GRAIN TRADE NEWS.

CANADA.

Grain shipments through Depot Harbor this season have aggregated 12,000,000 bushels.

The Canadian Northern Railway has completed track laying on its Gilbert Plains extension.

The Montreal Corn Exchange on Dec. 16 held a meeting and indorsed the Connors elevator scheme.

John Dilworth, of High Bluff, Man., is rebuilding his burned elevator at that place. The capacity of the new house will be 50,000 bushels.

Peter Campbell's grain elevator at Glenboro, Man., was burned Dec. 13, with 10,000 bushels of wheat. Insurance on building, \$5,000.

It is rumored that the Montreal Co. will purchase the elevator of the Moers Co., at Kingston, Ont., to provide facilities for handling the increased traffic, its own elevator being inadequate.

The Winnipeg Grain Exchange will request the department of inland revenue to instruct the grain inspectors to place on the certificate, issued by them for the inspection of grain requiring cleaning at Fort William, the exact amount of dockage to be made in order to have the grain take the grade defined by the inspector in the certificate issued. At present the Fort William elevator men, in cleaning cars, take an excessive dockage.

Special through rates on grain have been made effective by the Canadian Pacific Railway from points on the western division of the road to St. John's, Newfoundland, via St. John, N. B., and North Sidney, B. C. The new rates, per 100 pounds, are: Toronto, Hamilton, Peterboro, Brampton, Guelph, Galt, London, Woodstock, Ingersoll, St. Thomas, Chatham, Windsor, Preston, Welland, Brantford, Sarnia, Walkerville, Port Stanley, Niagara Falls, 32c; Fergus, Elora, 32½c; Orangeville, 35c; Mount Forest, Harrison, 33½c; Wingham, 34c; Owen Sound, 34½c. On grain from Owen Sound to Boston an eastbound freight tariff between the Canadian Pacific, the Maine Central, the Fitchburg road and the Boston & Maine has also become effective.

The Manitoba official crop report of Dec. 13 gives the yield of wheat for the province at 17.13 bushels per acre. The August estimate was 20.55 bushels per acre. The total wheat crop, on the reduced estimate of yield, is 27,922,230 bushels, instead of 33,504,000, according to the August report. The yield of oats is placed at 38.80 bushels per acre, and a total crop of 22,318,378 bushels. Barley 29.4 bushels per acre, and a total crop of 5,379,156 bushels. Flax 14 bushels per acre, and a total crop, 304,920 bushels. The area prepared for the next year's crop is 1,491,085 acres, or an increase of 480,680 acres over that reported a year ago. Of this, 861,070 acres is fall plowing, 158,515 is new breaking, and 472,500 acres of summer fallow. The open fall is accountable for some of the increase in land prepared for crop, the fall plow-

ing exceeding last year by 258,350 acres, the balance of the increase being in new breaking and summer fallow.

While in Chicago, Dec. 9, William J. Connors said: I have the support of all the large grain interests of Chicago and Duluth. The Montreal route will afford an all-water way to the sea, which can compete with the railroads because of its cheapness. Montreal is 300 miles nearer Liverpool than is New York, a decided advantage. Besides the three elevators at Montreal, we expect to build an elevator of 1,500,000 bushels capacity at Port Colborne, Ont., sixteen miles from Buffalo, and through grain going to Montreal in large cargoes will be broken there. During the season just ended only a little more than 21,000,000 bushels of the vast amount of grain which came to Buffalo from the west by lake and rail was shipped east by the Erie canal. The great bulk of it went by rail.

ILLINOIS.

James Cole, of Bushnell, Ill., was in Chicago this week.

McGive & Johnson, grain dealers at Elva, Ill., have sold out.

A grain and implement house has been built at Buckley, Ill., by Mr. Veatch.

Alex and Clyde Sornberger, it is said, will take charge of the Harms Elevator at Galesburg, Ill.

J. H. Newland, grain dealer at Farmer City, Ill., is now employed at the elevator of J. H. Williams.

Cole & Real, of Bradford, have purchased the grain elevator of Ryan & Harty at Wyoming, Ill.

E. W. Holt, of Knaul & Holt, Cincinnati, O., has bought the elevator of A. N. Gordy at Fithian, Ill.

Readers will confer a favor by sending us notices of new elevators, new firms and business changes.

James A. Cassidy, for 25 years a member of the Chicago Board of Trade, died recently at Phoenix, Ariz.

A new elevator at Essex, Ill., has been placed in operation. A gasoline engine furnishes power to drive the machinery.

The P. P. Williams Grain Co. is installing a large Fairbanks-Morse gasoline engine in its elevator at East St. Louis, Ill.

The West Elevator, which is being removed from Etherley, Ill., will be located at Copley, on land owned by Frederick Becker.

Henry Hemmelgarn, of H. Hemmelgarn & Co., grain commission merchants of Chicago, is spending the winter at Pasadena, Cal.

It is said that H. J. O'Neill will enter the employ of the American Maltng Co., as manager of the company's northwestern business.

R. J. Riley of Symerton, Ill., has bought the one-fourth interest of Mr. Dow, in the grain business of R. J. Riley & Co., at that point.

A small elevator, the second at that point, is being erected at Lee, Ill., by A. J. Prestegard, H. H. Hillison, and Lewis and Holden Resitter.

E. H. Ware, grain dealer at Douglas, Ill., is making some changes. The scales are being moved to a new location, and a new office is being erected.

C. H. Risser, of R. G. & C. H. Risser, grain dealers of Kankakee, Ill., has disposed of his interest in the firm's implement business to William Ohde.

H. J. Puterbaugh of Mackinaw, who operates elevators at that point and Lilly, has let the contract for the construction of an elevator at Menert, Ill.

J. M. Stauffer, of Symerton, has succeeded C. W. Green as manager for the Rogers Grain Co., at Gibson City, Ill. Mr. Green retires on account of poor health.

Best Bros. have purchased the interest of O. M. Best in the grain business at Palmer, Ill. The new firm is composed of Fay R. Best and Robt. E. Best.

A. Ellis, of Pana, Ill., has purchased of Mr. Wagner an interest in the grain and live stock business at Deer Creek, to which place Mr. Ellis will remove his family.

Knight Pearre will manage the new grain elevator of Geo. L. Kern at Dwight, Ill. Mr. Kern's elevator is modernly equipped, and the machinery is driven by a gasoline engine.

C. F. Bucher, the well known grain and live stock man of Freeport, Ill., and Brodhead, Wis., has entered into partnership with J. D. Rippey in the grain business at Stafford, Kan.

Fred Marquis of Aledo, Ill., was in Chicago recently. Having disposed of his grain interests at Aledo, he is looking for a new location in Iowa or Nebraska and may go to Galveston.

W. N. Shaw, grain inspector at Chicago, has been awarded a verdict for \$15,000 damages against the Erie Railway, for injuries. His right arm was cut off by the wheels of a freight car.

L. J. Keith & Son, Fairfield, Ill., Dec. 11: Corn is moving pretty freely at 26 cents. Wheat prospects are pretty bright for next season; that is, the growing wheat is going into the winter in good shape.

Since the meeting at Champaign, Admiral Dewey of Milmine, Bodman & Co., has been growing a beard. This daring change of front on the part of the admiral is sure to bring confusion to his friends.

E. J. Mosier, of Prophetstown, Ill., has bought a half interest in the elevator and grain business at Tampico, of H. E. Mammen, and will take possession Jan. 1. Mr. Mosier is a young man of good business ability.

One Walters, a reputed deputy grain inspector at Chicago, Ill., was a witness in the case of former State Grain Inspector Andrews. His evidence was so contradictory that the grand jury has indicted him for perjury.

James I. Best, southern manager for the Middle Division Elevator Co., of Chicago, died Dec. 24 at New Orleans, La. He was well known in the grain districts of Illinois, but since 1895 has resided at New Orleans.

M. O. Flanigan & Co., of Bellflower, Ill., inform us that they have sold their elevator at Bellflower and elevator at Kumler, together with cribs, fixtures and lumber yard, to Bateman & Noble Bros., in consideration of \$8,500.

Geo. M. Marshall, of Belvidere, Ill., was in Chicago last week and reported that the farmers were holding their corn, which is in good condition. The farmers of that vicinity are well fixed finan-

cially and intend to hold off for 30 cents.

Committees of the Chicago Board of Trade are still at work on plans of reform. No important changes have yet been adopted, though various radical revisions of the rules are strongly advocated by different members.

The firm of Slimmer & Lipman, Commission Merchants, Chicago, Ill., will dissolve partnership and retire from business Jan. 1. This firm has been in business six months, having succeeded Rheinstrom & Co. last July.

R. J. Riley, Symerton, Ill.: You may discontinue my advertisement of elevator wanted. I am getting elevators offered on all sides, and think I can get one out of the lot. Inclosed you will find stamps for advertisement.

A. M. Goff & Son, of Rantoul, Ill., have their new ear corn elevator in operation, in connection with a new crib of 15,000 bushels capacity, divided into several bins. If desired the corn can be delivered direct from the dumps to the sheller without being carried to the storage crib.

A. Yordy, employed at the new elevator at Shannon, Ill., was divested of his coat recently in a manner more rapid than agreeable. The governing device of the flywheel on the engine caught his coat. Mr. Yordy, with great presence of mind, braced himself and let the coat go.

Frank H. Peavey, of Minneapolis, Minn., the well known grain elevator operator, has purchased a controlling interest in the Chicago Cottage Organ Co. This company has a capital stock of \$2,000,000, half of which is preferred. The preferred stock has paid regular dividends of 6 per cent.

Work on the new Merritt elevator at South Chicago, Ill., was stopped recently by a strike, as a result of a misunderstanding between the owners' inspector and the contractor. To promote good feeling the inspector bought a keg of beer for the strikers, under the influence of which the matter was amicably adjusted and work resumed.

C. F. Austin, of Tonica, and R. I. Thornton, of Marshall County, have formed a partnership as Austin & Thornton to engage in the grain business at Gardner and Booth's Station, Ill., where they have purchased the business of H. E. Snyder. The latter has sold his new residence to Mr. Austin, who will remove his family to Gardner.

R. A. Sherman sold 200 puts for Seymour, Danne & Co., on the Chicago Board of Trade, and there being a loss of 1-4 cent, the firm repudiated all but 20, on the ground that puts and calls are illegal and cannot be enforced. Members of the board who are opposed to such trading are giving this case considerable publicity, with a view to discrediting the whole business.

Farmers had a meeting at Tuscola, Ill., Dec. 16, to form a grain shipping association. It was alleged the grain dealers have combined to keep down the prices offered for grain. C. C. Birdfield, of H. H. Carr & Co., Chicago, addressed the meeting on direct shipping. As a result a committee of five was appointed to incorporate the Hayes Grain & Coal Co., to build an elevator at Hayes, Ill.

John C. Schwartz will be deprived of his privileges as a member of the Chicago Board of Trade unless he settles a claim of \$540 held by Charles Van Kirk on account of an old wheat deal. The directors of the board decided that the claim was not wiped out

by the bankruptcy proceedings begun by Schwartz. During 1893 Mr. Schwartz cleared \$300,000 on a deal in May corn.

If all terminal markets would deal as promptly with grain thieves as Peoria, Ill., recently did with William Cantrell, shippers would have less reason to complain of shortages. Cantrell was caught in the act of stealing grain from a car in the yards of the P. & U. R. R. He was sentenced two days later to a term of eight months in the workhouse and to pay or work out a fine of \$50 and costs. Hurrah for Peoria.

Charles Horneman and C. A. Vincent, regular dealers at Odell, Ill., have indulged in an overbidding contest lately which has brought joy to the farmers and discontent to dealers at nearby stations. Next week, when the dealers have finished looking for the profits on 1899 business, there will be much grief in Odell. May it serve to bring them to an understanding which will insure them living profits as well as their neighbors.

A committee of the Chicago city council recommends the improvement of the Chicago River by the removal of center pier bridges. The lowering of the tunnels is shown to be feasible. If these changes are made by the city the federal government will dredge the river to a depth admitting the largest lake vessels. The contemplated improvement will add considerably to the value of the grain elevator property situated on the banks of the river.

Not in nineteen years has there been as much noise and revelry on the floor of the Chicago Board of Trade as on Saturday, Dec. 23. Miniature bonfires were lighted in different parts of the room, and a football was brought in and kicked about the hall amid an excited crowd. Toward the close Henry Vogle invited all hands to come and take refreshments at his expense. He led the way to the apple stand and bought \$4.19 worth of the luscious fruit.

Harry A. Angel, a broker on the Chicago Board of Trade, has been suspended for five years for reporting the purchase of 10,000 bushels of oats for a customer at 1-8 cent more than was paid. The fine was \$12.50. The suspicion that "skinning" of orders is practiced by some firms appears to be well founded. Mr. Angel was disciplined not because this was the first offense of the kind committed by pit brokers, but because a spasm of morality is agitating the Board.

Bulletin No. 3 of the self-styled "Committee of Publicity" of the Chicago Board of Trade made its appearance Dec. 19. It makes a sharp attack on the management, as follows: "It is the sense of the members that the present directory should resign; no one has any confidence in their sincerity. Driven to the corner, at the eleventh hour, they are desperately trying to redeem themselves by punishing young men who practiced methods that had become a recognized 'industry,' through the anarchy which this same directory had permitted to exist and in which they reveled, until it became intolerable to the members. No true reform will come to the board through a directory that has to be forced to do its duty, and then boasts that it will do it so hard that the members will get sick of reforms. If we permit City Hall politics to rule us; cheap knaves to ruin our business and cut our commissions to pieces; bucket shops to have di-

rect wires from the wheat pit; our speculative trading will degenerate into common gambling, and we place manhood below a temporary pecuniary gain, then we are lost and sink to the level of the poolroom and gambling-house."

Fire at Pierson Station, Ill., on the I., D. & W. R. R., destroyed the elevator of John Schroll, Dec. 8, together with 12,000 bushels of shelled corn, 500 bushels of ear corn and 1,500 bushels of oats. Loss, \$6,000; insurance, \$3,100. Mr. Schroll had just sold the property to G. S. Tarbox of Arcola; but the formal transfer had not taken place, and the loss falls on Mr. Schroll. Mr. Tarbox bought the property to be managed by his son-in-law, John Salmons, who has had experience in the grain business.

INDIANA.

Send us notices of new elevators, new firms and business changes.

Mr. King has placed modern machinery in his elevator at Talbot, Ind.

Mouch & Millikan have completed their new elevator at New Lisbon, Ind.

A. Gardner, grain dealer of Cottage Grove, Ind., will build an elevator next spring.

The Pierce Grain & Implement Co. has been incorporated at Union City, Ind., with \$20,000 capital stock.

Archibald Bros., Morocco, Ind.: We have quit advancing money to farmers. We store grain only 30 days.

C. A. Gardner of La Grange, Ind., has sold his coal business, but will continue to deal in grain at his elevator.

John and William Caowder of Bunker Hill have purchased the elevator of Strod Hays at Sulphur Springs, Ind.

A man from South Whiteley, Ind., will buy corn at Bippus for a time, and, if the business justifies, will erect an elevator.

Burglars made an unsuccessful attempt to rob the safe of William Sharpe, grain dealer and banker at New Paris, Ind., Dec. 22. The explosion of nitro-glycerine, which blew off the outer door, made so much noise that the citizens were aroused and exchanged shots with the gang.

W. T. McCrary, Kentland, Ind., Dec. 21: We will be very busy if the roads are in condition so that the farmers can haul. Much of the time during this month the roads have been in bad order, and there will be a grand rush whenever circumstances get favorable to the movement of the crop.

The elevator at South Bend, Ind., operated by the Churchill-White Co., of Chicago, as a cleaning, clipping and transfer house, in connection with smaller elevators on the I., I. & I. Railroad, will be equipped with four additional elevator legs and enlarged with an addition of 200,000 bushels capacity in the spring. The power plant is being doubled, giving 200 horse power.

An Indiana dealer says: I aim to charge interest on all money that I have out on grain, and find it a very good plan; and I do not know that it is violating any law, written or unwritten. I have always contended that each person could do as he wished about those matters, and if I had money to loan at 8 per cent interest, I would as lief loan it to a farmer friend on his grain as anywhere else. A person could not have better security, and I do not think it is in any way opposite to correct business principles to

accommodate them. I understand, however, plenty of small dealers object to this.

IOWA.

H. H. Riepe, of Sperry, Ia., will build a 20,000-bushel elevator.

The Northern Grain Co. will build an elevator at Wheelerwood, Ia.

William Stelter is no longer in the grain business at Mediapolis, Ia.

Frey & Mast have purchased the grain house of D. F. Berry at Noble, Ia.

E. I. Hawk has succeeded W. P. Brittain in the grain business at Nugent, Ia.

Duskin & Son have purchased the Clark Elevator and corn mill at Seymour, Ia.

O. L. Keith has purchased one-half interest in the elevator of A. D. Hays at Wavland, Ia.

The first state meeting of the Iowa grain dealers is proposed to be held about Jan. 15.

Jim Crawford, who conducted a scoop-shovel business at New London, Ia., has retired from business.

The elevator at Wyman, Ia., has been leased by Walter McCullough, who will take possession Jan. 1.

I. E. Jackson, of Union, Ia., has bought an elevator near Cedar Rapids, to which city he will remove in the spring.

H. J. Reynolds, of Nodaway, Ia., is building a 10,000-bushel elevator, for which Mr. Anthony, of Corning, has the contract.

Dunkelburger & King have succeeded O. L. Dunkelburger in the grain business at Nevada, Ia. They operate a 30,000-bushel elevator.

Contracts for the construction of three elevators of 20,000 bushels capacity, on the M. & St. L. southeast of Fort Dodge, Ia., have been let.

An elevator will be built at Mason City, Ia., by the Nye & Schneider Co., of Fremont, Neb. A coal business will be conducted in connection.

Readers will confer a favor and help to advance the interests of the regular trade by sending notices of new elevators, new firms and business changes.

The Otto Roller Mill Co., of Wapello, Ia., whose plant was burned in October, will not rebuild the mill; but in the spring will erect a 10,000-bushel elevator.

A number of elevators are to be built on the line of the Fort Dodge & Omaha Railroad, by the Western Grain Co., said to have its headquarters at Winona, Minn.

Geo. Ridgeway, grain dealer at Plymouth, Ia., has been taken into the custody of the United States Marshal on the charge of issuing bank checks for grain without attaching a revenue stamp thereto.

Beck & McClurkin have about completed their 18,000-bushel elevator at Marsh, Ia. A 20-h. p. Frost steam engine will furnish power to run the elevator and a Barnard & Leas sheller and cleaner.

At Elwell, Ia., the only regular dealers are M. R. Smith, who operates a 5,000-bushel elevator, and the St. Paul & Kansas City Grain Co., which has a station at that point with an elevator of 8,000 bushels capacity.

We are informed that H. Ovens, of Toledo, Ia., is not a regular grain dealer and is not entitled to track bids or market quotations. It seems that he has

been causing the regular dealers at Montour and vicinity some trouble.

L. L. Lawrence, of Estherville, Ia., has completed an elevator at Luzon, Ia., with 20,000 bushels capacity. Mr. Lawrence, who formerly operated an elevator at Armstrong, July last, purchased the elevator of McCord Bros., at Estherville.

Miles Doran, of Nichols, Ia., has started his new elevator, which with his corn cribs has 100,000 bushels storage capacity. It is furnished with a 60-h. p. Frost steam engine; No. 4 Barnard & Leas sheller; and No. 4 Barnard & Leas cleaner.

S. C. Culbertson and his uncle, Mr. McGregor, of Panora, have purchased the grain and lumber business of P. M. Vest at Jefferson, Ia. The latter will retain the coal business and his interests at Cooper, Ia. Mr. Culbertson will give his personal attention to the elevator at Jefferson.

The grain men of Des Moines, Ia., held a meeting December 12 and appointed a committee to take steps toward perfecting a state association with permanent headquarters at Des Moines. Some practical ideas on organization were advanced by D. Hunter, of Hamburg, and G. A. Stibbens, of Coburg.

The Grain Dealers' Union of Southwestern Iowa and Northwestern Missouri, Secretary G. A. Stibbens informs us, will hold its next meeting at the Grand hotel, Council Bluffs, Ia., Jan. 9, at 2 o'clock p. m. A good meeting is expected, and every dealer in the territory should make an effort to be present.

L. T. Aldinger, of Sutherland, Ia., has sold his elevator to the Marfield Elevator Co., Winona, Minn., which will take possession March 1. Mr. Aldinger has been elected to the office of county treasurer, and could not do justice to his grain business and perform his public duties also. He will remove to Primghar, Ia.

J. A. Carden and wife, of Winfield, Ia., president of the Southeastern Iowa Grain Dealers' Association, was in Chicago last week and reported much poor corn in his section of the state. Some of the farmers are selling at 10 and 15 cents a bushel and glad to get that. The good corn will be absorbed by the feeders, who are paying 30 cents a bushel. Few dealers will have any corn to ship.

The final estimate of the crops of Iowa, as prepared by J. R. Sage, director of the weather and crop service, places the yield of winter wheat at 226,000 bushels, against 3,168,000 bushels in 1898; and the spring wheat yield at 19,574,000 bushels. The yield of corn was 306,852,000 bushels; oats, 140,647,000 bushels; rye, 2,061,000 bushels; barley, 14,719,000 bushels; and flax, 1,597,000 bushels. The total value of all farm produce for the year was \$194,605,000.

Milmine, Bodman & Co., of Chicago, Ill., and the St. Paul & Kansas City Grain Co. are building elevators in northwestern Iowa. J. F. Younglove has the contract for both firms. The first mentioned is building a series of 20,000-bushel elevators along the line of the Marshalltown & Dakota Railroad, recently completed into Gowrie, Ia. Edward A. Armstrong, of Fort Dodge, is supervising the construction. The elevators of the St. Paul & Kansas City Grain Co. are being built on the Mason City & Fort Dodge Railway at Mason City, Burchinal, Thornton, Meservey, Cornelia, Eagle Grove, Vincent, Industry and Evanston.

KANSAS.

The new elevator at Birmingham, Kan., is nearly completed.

Report the grain trade news of your section for publication in this column.

W. T. Daniels has stone on the ground for the new elevator at Scottsville, Kan.

Carpenters have begun work on Mr. Seitz's new grain elevator at Anson, Kan.

Work is progressing rapidly on the new 10,000-bushel elevator at Fostoria, Kan.

An elevator is to be built at Marietta, Kan., by the Midland Elevator Co., of Kansas City.

Cortland Fleming and Wilfred Helm expect to engage in the grain business at Mound City, Kan.

H. L. Strong, Coffeyville, Kan.: Stop advertisement; don't need it any longer; engines sold. Herewith find draft.

Baker Bros., of Asherville, Kan., has bought the elevator of M. L. Marshall, who is now operating at Sumnerville, Kan.

D. Daikers & Son, Marysville, Kan.: We receive the Grain Dealers Journal regularly, and could not get along without it.

The Ryan Live Stock Co. is building an addition to its elevator at Leavenworth, Kan., to be one-half the size of the original.

W. W. Culver, of Great Bend, formerly state grain inspector, proposes to construct a grain elevator of large capacity at Wichita, Kan.

R. S. McNeer, the new grain dealer at Baker, Kan., will remove and engage in the business elsewhere, as he could get no favors from the Missouri Pacific.

The property of the White Cloud Mill & Elevator Co., at White Cloud, Kan., was sold by the sheriff Dec. 14 to John H. Lynds for about \$2,600. The property is valued at \$5,000 to \$7,000.

J. F. Wellington, Kipp, Kan., Dec. 18: Crops are looking well, and promise well for a large crop next harvest. Some report the Hessian fly; but there is no damage to crop as far as can be seen now.

The receipts of the Kansas State Grain Inspection Department for November were \$2,051; the expenses, \$2,811. In October there was a surplus of \$700. Chief Inspector McKenzie says that November was a bad month.

The grain dealers of southern and western Kansas held a meeting at Hutchinson, Dec. 7. About fifty were present. The meeting was the most successful yet held by this branch of the Kansas Grain Dealers' Association.

The Greenleaf-Baker Grain Co. does not operate an elevator at Centralia, Kan., as stated in a recent issue of the Journal. The only two elevators at that point are those operated by Denton Bros. and W. T. Buck, of Vliets, Kan.

A meeting of southern Kansas grain dealers was held at Wellington, Dec. 5, at the office of the Hunter Milling Co., to discuss the rate question and the grain situation. The experience of different dealers with shipments to Kansas City and New Orleans was compared; from which it appeared that weights were much better when shipped direct from Wellington or Coffeyville to New Orleans, without intermediate transfer. Among those present were B. F. Smith of Anthony, E. J. Smiley of Concordia, C. E. Munson of Coffeyville, J. C. Elvin of Danville, W. D. McKenzie of Kansas City, Kan., John Buck of Danville, Sam

Cole of Harper, Felix Hatfield of Rome, H. F. Probert of Bluff City, Wm. Astle of Haven, and C. M. Bennett of the Capital Elevator, Topeka.

J. D. Rippey, Stafford, Kan.: You can discontinue my advertisement of elevator for sale, as I have sold a half interest to C. F. Bucher, of Freeport, Ill. The sale was consummated through the advertisement appearing in the Grain Dealers Journal.

To fix the date for the excursion of the Kansas Grain Dealers' Association to the southern ports, Secretary Smiley is canvassing the members to ascertain their wishes. The time will be arranged to suit the majority, immediately after the annual meeting of the State Association in February or March.

MICHIGAN.

The machinery has been placed in the new elevator of Muchler & Truax at Bad Axe, Mich.

Aaron Switzer, of Flint, is considering the feasibility of building an elevator at Durand, Mich.

The Flint City Mills, Flint, Mich., will build an elevator and buckwheat mill of 30,000 bushels capacity.

John Wahl's elevator at Monroe, Mich., was burned Dec. 13, at 1 o'clock in the morning. The house, owned by the F. & P. M. Railroad, was full of grain, there being 3,000 bushels of oats, 11,000 bushels of wheat, and enough other grain to make 30,000 bushels. Mr. Wahl's loss is \$35,000, with \$18,000 insurance. He will rebuild.

MINNESOTA.

Charles Riebe's new elevator at Miles, Minn., has been completed.

E. Danielson & Co. have arranged to build an elevator at Clinton, Minn.

T. A. Olmstead, the pioneer grain inspector of Duluth, Minn., is dead.

At Kiester, Minn., an elevator will be erected, it is said, by the Nebraska Elevator Co.

Vessel owners are asking $4\frac{1}{2}$ cents for winter storage and transportation at Duluth, Minn.

The new elevator at Kiester, Minn., of the Minnesota Elevator Co., is nearly completed.

Fire at Minneapolis, Minn., Dec. 14, slightly damaged the office of the City Elevator Co.

An elevator is being built at Hallock, Minn., by the Farmer's Grain & Implement Co., of Litchfield.

Henry Walter has resigned his position as manager of the Farmer's Elevator, Bellingham, Minn.

The L. Christianson Grain Co. has bought the elevator at Belle Plaine, Minn., of Samuel Bowler.

William Carver and J. P. Brandon will have charge of the construction of C. E. Thayer's new elevator at Duluth, Minn.

Julius H. Barnes, connected with the Ames-Brooks Co., grain commission dealers, Duluth, Minn., is absent on a trip to Europe.

J. A. Todd, of Kenkel, Todd & Bettinger, grain dealers of Minneapolis and Duluth, Minn., was married recently to Miss True Roberts.

Farmers in the northwest had marketed 54 per cent of the last wheat crop Dec. 1. Much of the grain marketed is held in interior elevators.

J. A. Engelhart, of Redwood Falls, Minn., has leased a site on the Gulf & Manitoba Railroad, at Beaver Falls, on

which to erect a 35,000-bushel grain elevator.

An employe at Elevator E, Duluth, Minn., named Alfred Ecklund, was drowned recently while wheeling a load of screenings along the dock.

S. H. Jones, connected with A. D. Thomson & Co., grain commission dealers of Duluth, Minn., will take a month's winter vacation in the Bermudas.

Work on the new Peavy Elevator at Duluth, Minn., has reached a stage where it is predicted the plant will be ready to go into operation Feb. 15.

It is reported that the uncompleted malting plant at Red Wing, Minn., will be acquired and finished by the Standard Malting Co., headed by L. J. Merkel, of New York.

At Nerstrand, Minn., Hegnes' elevator has been purchased by the Sheffield Milling Co., of Faribault, Minn. A farmers' elevator is talked of at the same place.

The elevator at Madelia, Minn. formerly operated by Gregory, Jenison & Co., of Minneapolis, has been leased and will be conducted by Christensen & Henderson, local millers.

Chief Grain Inspector Reishus of Minnesota, on Dec. 18, dismissed seven employes of the inspection department. Politics seem to have converted the Minnesota department into a side show.

A. L. Foster, of Echo, whose new elevator at Redwood Falls, Minn., was torn down by the Northwestern Railroad, says that the contractor, E. H. Troman-hauser, of Minneapolis, will bring suit against the company.

D. A. McDonald & Co., of Minneapolis, have purchased the elevator property of R. A. Grams & Co., on the Great Northern, at Bird Island, Dumas and Hutchinson, Minn. Mr. Grams will act as agent for the new owners.

Two elevators will be erected on the Great Western at Faribault, Minn., one of 50,000 bushels capacity by the Sheffield Milling Co., and the other, of 25,000 bushels capacity, by Hutchinson & Stockton, known as the Faribault Roller Mill Co.

Grain shipments from the head of Lake Superior during 1899 were 43,549,000 bushels of wheat; 8,418,000 bushels of corn; 2,976,000 bushels of oats; 820,000 bushels of rye; 3,190,000 bushels of barley; and 9,283,000 bushels of flaxseed. Compared with 1898 these figures show increase in corn and flax and a decrease in all other grains.

The Woman's Home Society has instituted mandamus proceedings against the state railroad and warehouse commissioners to compel the grain inspection department to turn over all grain samples to the society, as desired by the grain commission men, to whom the grain belongs as consignees. Chief Inspector Reishus says the samples belong to country shippers; therefore, the department will appropriate them and use the proceeds, thus keeping the inspection and weighing fees, paid by country shippers, at a minimum.

MISSOURI.

Items of interest to the trade are gladly received and published in this column.

Fire at Sumner, Mo., Dec. 7, destroyed the elevator of E. D. Kellogg. Loss, \$4,000; insurance, \$1,000.

The St. Louis Merchants' Exchange will hold its annual election Jan. 3. Two tickets will be in the field.

Jacob Vis, a grain importer of Amsterdam, Holland, was in St. Louis Mo., recently, on his way to the Orient.

Clanning Taylor, bookkeeper for the J. A. Menefee Grain Co., Kansas City, Mo., is charged with having raised checks.

The Robinson-Danford Commission Co., of St. Louis, Mo., is building a warehouse and grain elevator to cost \$20,000.

The Chalfant-Burroughs Grain and Bran Co. will be dissolved, F. P. Chalfant continuing in the grain business at Kansas City, Mo.

Baker & Co., of St. Louis, Mo., who claimed to be grain commission merchants, and dealt in options, have failed, causing loss to many country customers.

Gustav Eichholz, of Eichholz & Loeser, grain dealers of Hamburg, Germany, was in St. Louis recently, on his first trip to America. He will visit the principal grain markets.

Detailed plans are being prepared by a committee of the St. Louis Merchants' Exchange for the weighing of all grain, hay and feed, except that consigned to public elevators, under the supervision of the Exchange.

At the meeting of the National Board of Trade, in January, the St. Louis Merchants' Exchange will be represented by Oscar L. Whitelaw, H. F. Langenberg, E. O. Stanard, Selden P. Spencer and Breckenridge Jones.

N. J. Edmonds & Sons' elevator at Marshfield, Mo., was burned Dec. 7. The plant was rented to Jameson, Buchfield & Smith, who had 100 bushels of wheat and 600 bushels of corn destroyed. Edmonds' loss is \$2,600, with no insurance.

At a meeting of the executive board of the Kansas Grain Dealers' Association, at Kansas City, Mo., Dec. 14, it was decided to continue the check weight bureau. Expenses will be reduced by cutting down the force of check men from 10 to 6. As grain receipts increase the force will be increased. The expense of conducting the bureau has been \$600 per month.

McReynolds & Co., of Chicago, will operate the Sun Elevator at Kansas City, Mo., after Jan. 1. Their office in Kansas City will be in charge of G. H. Conant, now in charge of their Omaha office. Friends of Mr. Conant are pleased with this recognition of his ability by McReynolds & Co., with a deserved promotion.

The new directors of the Kansas City Hay Dealers' Association met Dec. 14, and elected: President, J. A. Menefee; vice-president, B. F. Tyler; secretary, J. T. Woolsey; treasurer, Bert Sheldon; arbitration committee, E. T. Adair, E. R. Boynton, and H. J. Schilling. Chairmen of committees: Transportation, J. A. Brubaker; finance, B. F. Tyler; weights and scales, J. I. Huffine; inspection, A. E. Chesterfield; constitution and by-laws, J. B. Robison.

Directors of the St. Louis Merchants' Exchange, at a recent meeting, declared exchanges were being discriminated against in the war tax of 1 cent on all sales of \$100, and congress will be asked to make it applicable to future deliveries outside of exchanges as well. Chicago's request for co-operation in stamping out the bucket shop was

heartily favored, and it was agreed to send delegates to a convention for the purpose if Chicago called one.

The Missouri state crop report places the yield of corn at 183,000,000 bushels, against 190,000,000 bushels in 1898; and the yield of oats at 20,000,000 bushels, against 16,600,000 bushels in 1898. The wheat acreage seeded in the fall of 1899 has declined 4 per cent, as compared with last season, leaving an area of 1,000,000 acres. The decrease was greatest in the southwest section, amounting to 8 points, where, for want of moisture, it was with difficulty that a proper seed bed was prepared. There was a general complaint over the state of want of moisture for proper preparation of the ground, but, with a little extra effort, the seeding was usually done in a satisfactory manner, and the plant has made a fair growth.

NEBRASKA.

The new elevator at Brady, Neb., has been completed.

Albert Upton of Tobias has bought an elevator at Fairbury, Neb.

Bossemer Bros' elevator at Superior, Neb., is rapidly nearing completion.

C. J. Worrall, of Pickerell, Neb., has recently raised his elevator and improved and remodeled it, making a first-class house out of it.

Ed. Wenzel & Son, of Eagle, Neb., write that they are thinking of enlarging their elevator and putting in a gasoline engine and a feed mill.

At Fremont, Neb., the Omaha Elevator Co. has about completed its new elevator and out-buildings. The company is building a crib 200 feet long.

E. E. Clancy, of Beatrice, Neb., will take charge of the Omaha office of McReynolds & Co., of Chicago, succeeding G. H. Conant, who goes to Kansas City.

The Dowling & Purcell Co. has been incorporated at North Bend, Neb. Capital stock, \$50,000; incorporators, M. Dowling, T. B. Purcell and H. P. Dowling.

Kyd & Co., of Beatrice, this summer built an elevator at Putnam, a switch between Beatrice and Blue Springs; also others at Rockford, Odell and Burchard, Neb.

E. Loosbrook, of Pittsburg, Neb., has a modern elevator which has been running about three months. But as yet the railroad has not put in a side-track for him.

The Farmers' Elevator Co. has been incorporated at Holdrege, Neb., to deal in grain and live stock. Capital stock, \$6,000; E. H. Cannon, president, and A. Safley, secretary, both of Holdrege.

The Omaha Commercial Club will endeavor to stop the circulation of lying reports about the condition of winter wheat in Nebraska. The periodical fake stories that are sent to the eastern newspapers are very damaging to the business interests of the state.

Secretary A. H. Bewsher of the Nebraska Grain Dealers' Association in his Dec. 15 crop report says: The fall acreage of the state, it is believed, will be about 35 per cent less than that sown a year ago. The acreage at that time having been reported, as shown in our report, as 1,061,675 acres. Considering this 35 per cent deduction, we would have an area of about 700,000 acres now planted to winter wheat. This acreage is reported as 21 per cent greater than that harvested this summer.

NEW ENGLAND.

A vaudeville entertainment will be given on the last day of the year by the members of the Boston Chamber of Commerce.

E. S. Smith has opened a grain store at West Bethel, Me., in a building more than one hundred years old, and still in good repair.

A four-story grain elevator is being erected at Hartford, Conn., for the Daniels Mill Co. The building is of brick, 74x50 feet.

Edward P. Merrill, grain broker, Portland, Me., writes: Grain dealers here think corn and oats at present prices are good property. The trade is using corn feed and mill feed lightly. Hay is coming to market slowly. The railroad freight rates need fixing in some far-reaching and just way. Portland, Me., as a point for departure for Europe is coming into favor. December this year, as usual, is a month of settlements and stock taking.

NEW YORK.

The Lackawanna Railroad has acquired property at Brooklyn, N. Y., on which to erect freight terminals on a grand scale.

The farmers of Broome County, N. Y., have organized a combine for the sale of their products, and will have headquarters in New York City.

The New York State Commissioner of Agriculture reports farming operations in that state for the current year the most profitable for many seasons.

The committee on canals of the New York Produce Exchange has issued a pamphlet on canal enlargement and a canal map, embracing the territory between Nova Scotia and St. Paul and Lake Superior, and the north of Kentucky, prepared under the direction of the committee. This pamphlet presented a summary and report on the diversion to other ports of inland commerce tributary to the port of New York, the relation of the railroads to this diversion, and the practical means to restore to New York the commerce of the interior and permanently establish its pre-eminence in trade.

NEW JERSEY.

Howell's elevator at Morrisville, N. J., will be operated by the Trenton Mills & Elevator Co., of Trenton, N. J.

Charles H. Spriggs, who sold his elevator to I. J. Dickson, has begun the erection of another warehouse and elevator at Washington, Pa.

The firm of Sitley & Son has been incorporated at Camden, N. J., to deal in grain. Capital stock, \$125,000; incorporators, F. B. Sitley, J. P. Warr, Jr., G. E. Taylor, C. T. Sharpless, all of Camden.

NORTHWEST.

Northwestern farmers are eager to plant more flaxseed next summer.

R. E. Whittemore will erect an elevator at Drayton, N. D., next spring.

An unknown kind of bug has been found in the grain bins at Columbia and Ordway, S. D.

A flax mill will be built by the people of Enderlin, N. D., who are forming a company for the purpose.

The Thorp Elevator at Grafton, N. D., has been leased by W. C. Liestikow, who has placed Mr. Bullock in charge.

N. P. Rasmusson, of Fargo, N. D., has let the contract for his new elevator at Dazey. He will also erect a dwelling for his agent.

OHIO.

W. D. Rapp has succeeded M. C. Gray in the grain business at Sabina, O.

Rhinehart Smith, Sidney, O.: Certainly I want the Grain Dealers' Journal.

L. A. Pugh, of Mark Center, O., will build a grain elevator provided the citizens of the township give him \$1,000 bonus.

A. Groweg, of East Defiance, O., has installed a new steam boiler in his elevator and will make other repairs costing \$500.

J. W. Fetter, of Tamah, O., has his elevator completely filled with corn and small grain. He has 12,000 bushels of corn on hand.

Geo. McCabe, of Southworth & Co., grain receivers of Toledo, O., has recently purchased a fine residence in that city. The price paid was \$10,500.

The Baltimore & Ohio Railroad Elevator at Sandusky, O., operated by Rosendaum Bros., of Chicago, is to be enlarged to 1,000,000 bushels capacity.

O. P. Lenox, of Richwood, O., informs us that he will build an elevator of 20,000 bushels capacity next spring. The equipment will be modern and include an ear corn dump.

Royce & Coon have arranged to erect a grain elevator costing \$8,000 at Cygnet, O., on the ground donated by Percy H. Burnell, of Toledo.

The grain dealers of northwestern Ohio held an executive session at Lima, Dec. 18, to fight the increase in freight rates and to prevent the threatened demoralization of their business.

R. P. Barrett informs us that the Leesburg Grain Co., of Leesburg, O., expects to build an elevator early the coming spring. The capacity will be 25,000 bushels, and the machinery will be new.

Cincinnati grain dealers will petition the railway car service bureau to change its rules in favor of the small dealers. Demurrage accrues at the rate of \$1 per day after the first 48 hours. Many small dealers lack the facilities to unload a large carload within 48 hours. It is desired that an additional 24 hours be allowed on cars exceeding 35,000 pounds.

PACIFIC COAST.

Grain houses at Tacoma, Wash., are full of wheat.

T. T. Black, grain dealer at Portland, Ore., is dead.

Readers will confer a favor by reporting the grain trade news of their part of the country.

The Northern Pacific will build seven new elevators on its line in Washington, Idaho, and North Dakota.

The Galbraith Grain Co., of Seattle, Wash., has put in one of the largest hay presses used on the Coast. Improvements costing several thousand dollars are being made by the company, whose trade has grown rapidly.

On the grain bought this season by the Waco Warehouse Co., at Columbus, Wash., amounting to 250,000 bushels, the farmers paid 5 cents a bushel, or \$12,500, for ferriage across the Columbia River. The farmers are agitating for better transportation facilities.

The grain trade of the Coast is suffering from a lack of ships. Terminal elevators are filled with wheat which



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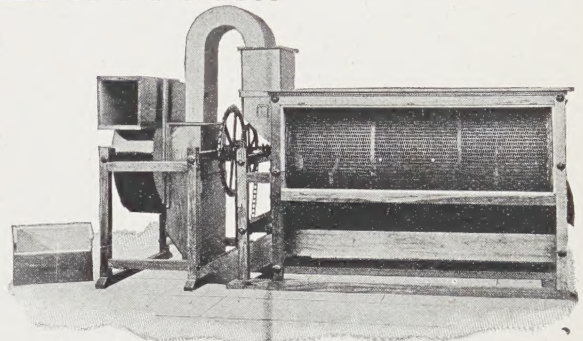
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on Modern elevators and Contents can be
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Insures only desirable risks in Iowa, and has saved its
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All business placed in Old Line Companies, only
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Seamless Baskets.

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Absolutely prevents mixing
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Absolutely prevents all dust and obnoxious
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It is small, but effective, and will never wear
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I buy damaged grain of all kinds. Write or wire me.

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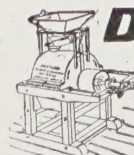
DOCKAGE TABLES...

Show the amount of dockage at 1½ lbs.,
2 lbs., 2½ lbs., 3 lbs., 3½ lbs., 4 lbs. and 5 lbs.
per bushel for any sized wagon load of wheat
containing from 2 to 600 bushels. This table
is constructed to conform to the usual cus-
tom of dropping all odd pounds in the weigh-
ing of wheat. It is printed on strong card-
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Price, 10 cents each.

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Don't feed your grain whole when you can
save half by simply grinding it. Much
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All ground grain is wholly digested. Our

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prepare family meal and grind all grains
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SEAMLESS COTTON GRAIN BAGS. MILWAUKEE BAG CO. MILWAUKEE, WIS.

cannot be moved out under present conditions. The increase in ocean freights has caused a decrease in the price paid to the farmer.

Several of the local grain dealers at Tekoa, Wash., have formed a pool, hoping by concerted action to dispose of their wheat to better advantage than by individual sales. In the pool were 40,000 bushels of wheat, of which Sparks Bros. own 23,000, A. Cohn 7,000, and various parties the remainder.

Creditors of Hiestand, Warner & Co., large grain dealers with headquarters at Oakesdale, Wash., held a meeting recently to devise means to tide the company over its temporary financial embarrassment. The assets are more than double the liabilities. Last spring the Hiestand-Warner Warehouse was formed to carry on the grain business separately from the merchandise business.

A. G. McAdie, director of the California section of the United States weather bureau, says in his report for the week ending Dec. 11: In the northern portion farm work is progressing slowly, owing to continued rains, but in the San Joaquin Valley and Southern California plowing and seeding are progressing rapidly, and an unusually large acreage of grain is being sown. In all sections early sown grain continues thrifty, and in some localities the later sown is up and looking bright. Green feed is more abundant than has been known for several years, and stock is in excellent condition.

SOUTHEAST.

Louisville grain dealers are seeking relief from the Big Four Railroad, which, they claim, is granting some sort of concession to certain concerns in Louisville, Cleveland and St. Louis, whereby their bids are higher for corn in the country.

Secretary R. S. Weaver of the Grain and Hay Association of Norfolk, Va., informs us that his association has adopted a resolution instructing the secretary to inform the secretary of every organization of any member of the Norfolk association who is expelled for unbusiness-like dealings. The Norfolk association will take up any trouble between one of its members and a member of any other association for adjustment. It is unfortunate that the reputable dealers of Norfolk should find it necessary to take such action in order to protect their businesses. Norfolk seems to be cursed with a few receivers who are disposed to deal unfairly with shippers.

SOUTHWEST.

Dennis & Lovejoy, of Berthoud, Col., are making improvements at their feed mill.

The growing wheat in the southwest is in good condition and is affording pasturage to herds of cattle. Farmers are busy delivering wheat and corn.

Cy. Williams, Norman, Okla.; You may stop my advertisement for the present, as I am getting quite a number of letters of inquiry, and think I will likely make a sale from some of them.

Cy. Williams, Norman, Okla., Dec. 23: We had a three-inch snow on the night of the 21st, but all melted away on the 22d, and to-day is like a spring day. Our wheat continues to look very flattering. Pauls Valley, I. T., is to have

the coming season a corn plant that will consume 10,000 bushels of corn per day in the manufacture of starch, grits, flakes and meal.

Receipts of rice at New Orleans during the period from Aug. 1 to Dec. 1 were as follows: Rough rice, 713,000 sacks, compared with 459,223 for corresponding time in 1898; the shipments were 631,286 sacks, against 405,723 sacks; receipts of cleaned rice, 3,865 barrels, against 894 barrels; sales, 147,777 barrels, against 105,689 barrels; stock on Dec. 1, 110,631 sacks rough and 21,416 barrels cleaned rice.

The Southern Pacific railroad, which recently raised its rates on wheat from \$4 to \$7.60, has restored the former figure for transportation from Ogden to San Francisco. At the high rate of \$7.60 per ton, the farmers of Utah found that it would pay them better to send their grain northwest over the Oregon Short Line, or southwest over the Texas railroads. An additional inducement to restore the low rate was the improved car supply.

TEXAS.

Bowman & Wall, Plano, Tex.: The Journal is certainly a fine paper for grain dealers.

A neat little calendar is being sent to its friends by the Texas Star Flour Mills, who operate a public grain elevator at Galveston.

Frank Kaucher of St. Joseph, Mo., is said to have the contract to build the 300,000-bushel elevator of the C. F. Orthwein's Sons Grain Co., at Fort Worth, Tex.

The Texas Railroad Commission again is contemplating making an emergency maximum rate of 8 cents on oats between Texas points. Four of the railroad companies have failed to make good their promises to restore the 20-cent rate to Mississippi river crossings.

It appears that the Texas oats rate matter will not down. On account of his letter attacking the State Railroad Commission, that body has summoned J. L. J. Polk, general manager of the Gulf, Colorado & Santa Fe, to appear and show cause why he should not be adjudged guilty of contempt of court.

John Reymershoffer, who built the first large grain elevator at Galveston, Tex., died suddenly in New York city, Dec. 12. He was troubled with Bright's disease, but apoplexy was the cause of death. Mr. Reymershoffer was one of the best known men in the grain and milling trade of Texas. Born in Austria, he came to the United States in his youth, and for years represented his country as consul at Galveston. In 1879 he built the Texas Star Mills, of which he was president at the time of his death. The news of his death was received in Galveston with great sorrow, and the flags on the consulate and public buildings were put at half mast.

In order to remove the possibility of misunderstanding or confusion in regard to the exception in section 5 of the commodity tariff, No. 2-A, which provides rates for the transportation of grain and grain products over the Texas, Sabine Valley & Northwestern Railway, the Texas Railroad Commission finds it necessary to state that shipments of corn in straight carloads, and of oats, in straight carloads, transported over the Texas, Sabine Valley & Northwestern Railway are subject to the rates and provisions embraced in

section 4 of the tariff, as amended, without reference to the exception above named. When corn and oats, in mixed carloads, are shipped over the Texas, Sabine Valley & Northwestern Railway, then they are subject to the rates and provisions specified in the exception.

WISCONSIN.

William J. Culbertson has succeeded Orndorff & Culbertson in the grain and stock business at Delavan, Wis.

Mrs. B. T. Williams has succeeded Walker & Williams, dealers in grain and implements at Boscobel, Wis.

A bucket-shop operator at Appleton, Wis., departed one night recently with his dupes' money, leaving behind a number of unpaid bills.

J. Evans, Barneveld, Wis.: It is a pleasure to look over the Journal when it comes to hand. It gives a good deal of information to grain dealers.

J. Evans, Barneveld, Wis.: There is not much grain to be shipped out this year, as the farmers are going into stock, and will feed the bulk of the grain.

Since rebuilding the burned elevator at Picketts, Wis., M. B. M. Peacock has had a good business. About 1,000 bushels of grain are received and shipped daily.

A grain elevator of 20,000 bushels capacity is being erected at Spirit Falls, Wis., a new town, in connection with a flour mill. The machinery is supplied by the Edward P. Allis Co.

Smith & Co., of West Superior, will erect a flat house at Truax, Wis., for potatoes and miscellaneous merchandise, and in the spring will erect a grain elevator. Samuel Fowler will be in charge.

The Northern Grain Co., of Manitowoc, Wis., is preparing to store large quantities of grain in its elevators and on boats in the harbor, several large steamers having been chartered for winter storage.

The directors of the Milwaukee Chamber of Commerce, in reply to a request from Secretary Stone, have advised the Chicago Board of Trade that the way to stop bucket-shopping is to prohibit the posting of quotations on blackboards in any broker's office.

The elevator at Washburn, Wis., on Dec. 3 loaded one of the largest cargoes on the great lakes. The cargo was loaded into the steamer H. W. Oliver for shipment to Buffalo. It consisted of 152,000 bushels of barley, 40,000 bushels of rye, and 149,000 bushels of oats; total, 341,000 bushels.

Clark & Taylor, dealers in grain and produce at Chippewa Falls, Wis., were given a handsome write-up in their local paper recently. The firm is well equipped for the business, having grain elevators of 10,000 bushels capacity, cold storage warehouse, and large potato cellar. An electric motor drives the grain separator and feed mill.

The Standard Malting Co., of which L. J. Merkel of New York is at the head, will build a malting plant with a capacity of 1,000,000 bushels in Milwaukee, Wis. The plant will cost \$600,000. It is said the capital will be furnished almost exclusively by eastern brewers. The Standard Malting Co. is not connected with the trust. The proposed plant will be operated in connection with one which has just been erected at Red Wing, Minn.



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A number of new factories and industries have been induced to locate—largely through the instrumentality of this Company—at points along its lines. The central position of the States traversed by the Chicago, Milwaukee & St. Paul Railway makes it possible to command all the markets of the United States. The trend of manufacturing is westward. Confidential inquiries are treated as such. The information furnished a particular industry is reliable. Address **LUIS JACKSON**, Industrial Commissioner C. M. & St. P. Ry., 660 Old Colony Building, Chicago, Ill.

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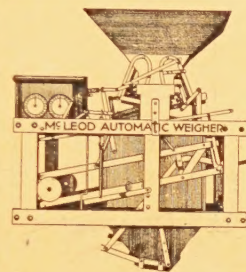
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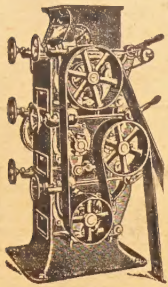
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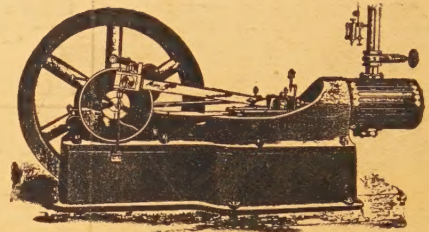
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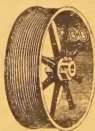
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